



HARVEST LANDING

SPECIFIC PLAN

ADOPTED MAY 10, 2011
ORDINANCE NO. 1276

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Section 1:

INTRODUCTION



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SECTION 1: INTRODUCTION

Project Summary

Harvest Landing is designed as a master-planned community on 341 acres in western Perris. The community will contain a variety of residential housing types, businesses, and amenities all integrated by a system of open spaces and paseos. The signature feature of Harvest Landing is its open space amenities; key among those are the Harvest Lake and surrounding Harvest Lake Park. Additional recreational amenities include an active community park, recreation center, and paseos.

The Harvest Landing Specific Plan serves as the long-range plan for the property and the guide to all future development within the Specific Plan area. This document will guide the build-out of the property in a manner that is consistent with City and state policies and standards and assures that the various community elements identified in the Specific Plan will be developed in a coordinated manner.

This Specific Plan provides detailed text and exhibits describing the various land uses, amenities, and infrastructure improvements that are envisioned to occur in Harvest Landing.

As of the writing of this Specific Plan, the project site was owned by the Coudures family, who have farmed and inhabited the property since the 1920s (see sidebar). As long-term residents of the Perris Valley, the Coudures family desires to be stewards of the property and create a legacy that honors their name and the City of Perris. The Harvest Landing Specific Plan is the ideal method of ensuring that the Coudures family's vision of a healthy lifestyle, long-term quality, and rich design is realized.

Project Site

As shown in Figure 1-1, *Regional Location*, Harvest Landing is situated in the City of Perris on the western edge of a broad valley formed by the San Jacinto River. Surrounding major physical and topographical features include Lake Perris and the Bernasconi Hills to the northeast, the San Jacinto River to the east and south, the

THE COUDURES FAMILY

Marie Lassa and Jean Marie Coudures immigrated to the United States from France in 1903 and 1910, respectively. Jean went to work at a sheep ranch near Alessandro, California, and met Marie, who was a cook there. They were married in Riverside in 1915 and began farming in Alessandro with a mule team. They had four children: John Jr., Louise, Denise, and Marie.

In 1928 they moved their home and ranch headquarters to property at Perris Boulevard and Morgan Street in Perris. Here they continued farming and raising sheep until the 1960s. John Jr. had a new home built for Marie in 1965 on Indian Avenue in Perris.

Over time, Jean and Marie purchased more land in the Perris area, eventually accumulating several thousand acres. At one time they farmed approximately 15,000 acres in alfalfa, potatoes, and sugar beets. John Jr. worked closely with his parents in the farming operation and continued to manage it after his parents passed away in the 1970s.

The 1980s found the family easing out of active farming and leasing the bulk of their land to sod farmers. In the early 1990s, Interstate 215 was constructed through the area and John Jr. took advantage of the new freeway off-ramp and developed the Perris Plaza shopping center on family land at Perris Boulevard and Nuevo Road.

Although John Jr. passed away in 1995, the family retains the bulk of the land acquired by Jean and Marie. Third-generation family members actively manage Perris Plaza and other Coudures family holdings.

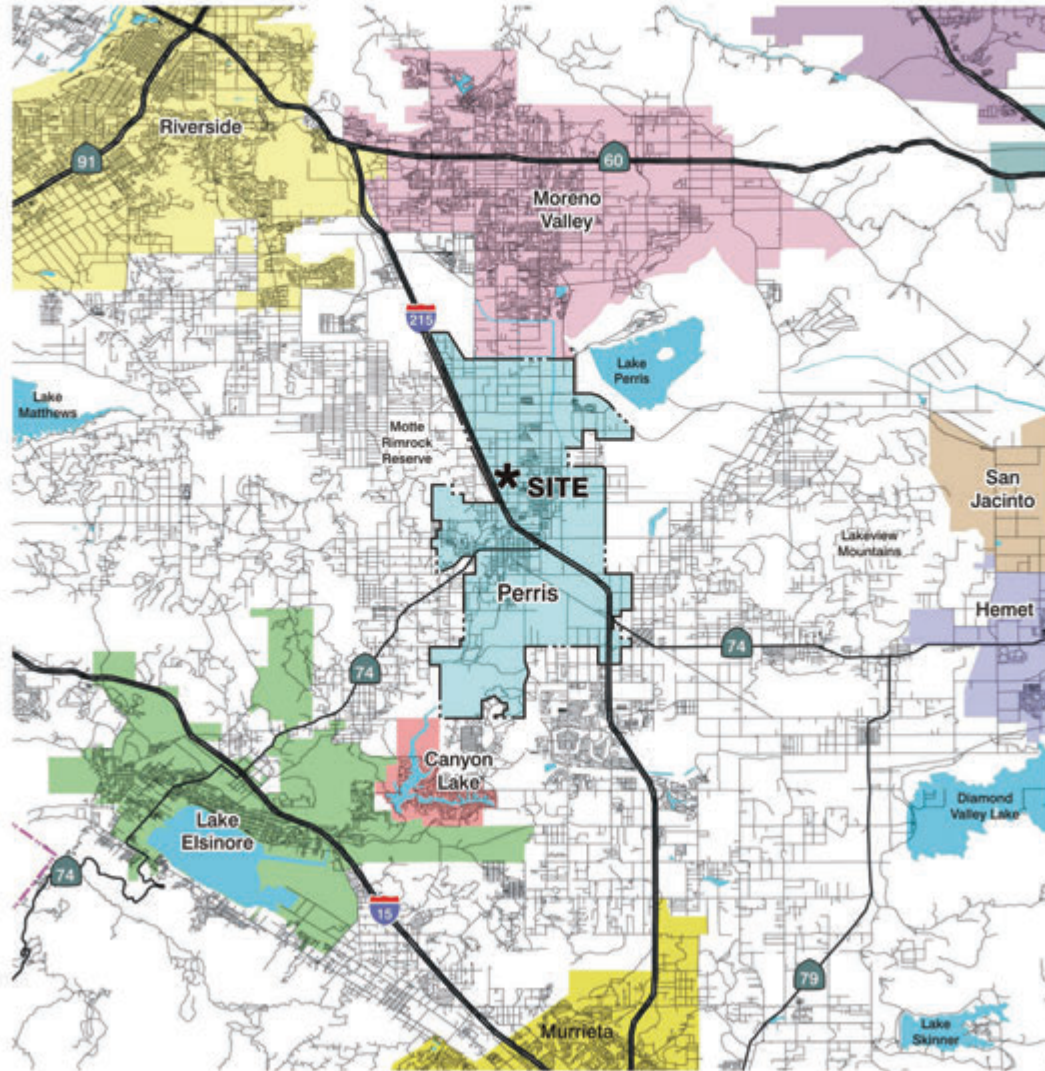
Lakeview Mountains to the southeast, and the Motte Rimrock Reserve to the west.

As shown in Figure 1-2, *Local Vicinity*, Harvest Landing is generally bounded on the north by Placentia Avenue, on the south by Nuevo Road, on the east by Perris Boulevard, and on the west by Interstate 215 (I-215) and East Frontage Road.

Harvest Landing is ideally situated to take advantage of nearby transportation facilities, including I-215, which runs along the western edge of the project site, and California State Highway 74 to the south of the site. March Air Reserve Base, part of which is slated to be converted into a major commercial airport called the March Global Port, is located to the north of the project area. Perris Valley Airport, located to the south of the project site, provides for smaller aircraft traffic.

Primary access to Harvest Landing would be from Perris Boulevard, Placentia, Indian, and Barrett Avenues, and Nuevo Road, which provides access to I-215. Indian Avenue traverses the site from north to south and Orange Avenue traverses the site from east to west.

REGIONAL LOCATION



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LOCAL VICINITY



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Vision and Guiding Principles

The City of Perris General Plan, which was comprehensively updated in the spring of 2005, describes an urban use for the project site. The goal of this plan is to meld the various land uses described in the 2005 General Plan into a unified village that capitalizes on its proximity to I-215 and is grounded in market realities.

Vision

Heart and Soul

Harvest Landing must have a “heart and soul,” or central feature, that makes it a community in the truest sense of the word. Accordingly, Harvest Landing must have a unified open space structure, clear theme, distinctive recreational amenities, and ample places to gather, socialize, and play.

To accomplish this, there must be a unifying feature. This crucial feature will be a system of open spaces and recreational features, surrounding a signature lake. Paseos and green spaces will tie numerous recreational amenities and various neighborhoods together.



The type of lake feature and surrounding park envisioned for this plan becomes the central, identifying feature of a community.



A range of lifestyle choice will be available in Harvest Landing.



Office, commercial, and light industrial uses are accommodated in Harvest Landing.

Range of Lifestyle Choices

Harvest Landing must offer a mix of residential selections that accommodates a range of the market spectrum, including first-time buyers, young singles and couples, families, empty-nesters, and seniors. This means that Harvest Landing will include residential choices ranging from detached residential homes, small-lot detached homes, contemporary townhouses, condominiums, and apartments. This range of housing types diversifies the project and ensures its long-term stability.

Flexible Business Options

Harvest Landing must be rooted in market realities. The business uses that are accommodated must be based on a careful consideration of the surrounding uses and market. The existence of major retail centers and the pending development of additional retail uses dictate that additional commercial retail uses may not be viable in the long term. Instead, a unique blend of professional offices, light industrial, research and development, business park, and retail commercial uses, which take advantage of the adjacent I-215 and complement the surrounding retail uses, will help build a robust, employment-based economy in Perris.

Unique Identity

It is the vision of this Plan to create an identifiable place—the kind of place that people want to experience and be a part of. To accomplish this, there must be the strong open space system discussed above. In addition, there must be an attention to detail that sets this place apart from other planned communities. Rich landscaping, streetscaping, and architectural features must be included in every aspect of the plan. Identifiable entry features, unified lighting, and community signage will add to the unique character of the place.

Guiding Objectives

To achieve the vision, the following objectives will serve as the guiding objectives for Harvest Landing:

General Plan Consistency—Base the future development and use of Harvest Landing on the vision and concepts of the City’s General Plan. However, refine the General Plan land use designations as they pertain to this site based upon market realities and sound design principles.



Quality of Life—Design Harvest Landing with an array of amenities such as water features, distinctive neighborhood identity, and active and passive parks, to ensure a high quality of life for residents, employees, and visitors.

Balance—Design Harvest Landing to provide a balanced mix of commercial, business, and residential uses that will complement the surrounding neighborhoods and create a viable community.

Community Design—Establish a strong community identity through the integration of design and architectural standards in the Specific Plan and a rich pattern of landscaping, streetscaping, signage, and architecture to create attractive, walkable, and distinctive neighborhoods.

Recreation—Provide local areas for active and passive recreation that are accessible by an integrated trail system and comply with the City’s Park and Recreation Master Plan.

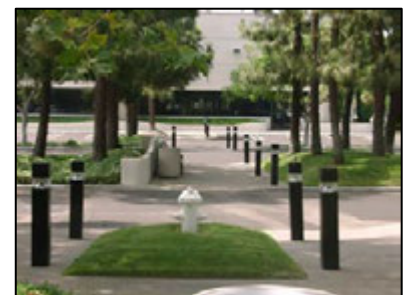
Economic Viability—Ensure the economic viability of Harvest Landing by creating a modulated site design that can be built at different times and by different developers. Allow for flexibility to best meet market demand at the time of development.

Connections—Organize land uses to promote pedestrian-oriented circulation patterns and reduce the number and length of vehicular trips.

Employment Opportunities—Enhance the City’s existing job base through the creation of a broad range of employment and career opportunities within Harvest Landing.

Housing Opportunities—Provide a wide variety of housing types, densities, and designs that accommodate a broad range of income levels and lifestyles and respond to both local and regional housing needs.

Responsive Plan—Develop a plan that responds to and minimizes the impacts of the potential Mid-County Parkway. Refer to the Mobility Plan in Section 3, *Development Criteria*, for detail on the proposed Mid-County Parkway.



Examples of the types of pedestrian amenities and connections that are desired in the residential and business areas of the community.

Purpose of the Specific Plan

As provided in California Government Code, Section 65450, a specific plan has been prepared for the systematic implementation of the updated General Plan. As provided for in state law, a specific plan is a regulatory document that provides standards and criteria for the development of a particular geographic area. The Harvest Landing Specific Plan establishes the planning concept, design and development guidelines, administrative procedures, and implementation measures necessary to achieve the orderly and compatible development of the project area. It is also intended to maintain consistency with and carry out the goals, objectives, and policies of the City of Perris General Plan.

The City of Perris Municipal Code, Section 19.49, specifies the purpose, requirements, regulations, and procedures for preparation of a specific plan in the City. California Government Code, Sections 65450 through 65454, establish the authority to adopt a specific plan, identify the required contents of a specific plan, and mandate consistency with the General Plan.

The California Government Code states that a “specific plan shall include a statement of the relationship of the Specific Plan to the General Plan, and further, that it may not be adopted or amended unless found to be consistent with the General Plan.” The consistency of the Harvest Landing Specific Plan with the City of Perris General Plan is addressed in Appendix B of this document.

Format of the Document

The Harvest Landing Specific Plan is divided into the following sections:

Section 1: Introduction—This section provides the definition of and authority to prepare a specific plan, the format of the document, and a description of the location, opportunities, and constraints of the Harvest Landing site.

Section 2: Development Plan—This section describes the proposed development concept and includes the land use plan and build-out statistics.



Section 3: Development Criteria—This section provides the detailed descriptions of the land use plan, land use designations, development standards, statistical data, circulation plan, open space plan, and utility and infrastructure plans.

Section 4: Design Guidelines—This section provides the Design Guidelines, which direct the type, style, and design of development in Harvest Landing.

Section 5: Implementation—This section contains the provisions for development processing, the development administration system, and the phasing program for the Harvest Landing Specific Plan.

Appendices—The appendices contain the landscape plant palette, definitions, general plan consistency analysis, market analysis, and infrastructure analysis. The water, drainage, and sewer master plans and environmental impact report are contained under separate covers.

Action Expectations

Numerous statements occur in this plan in the form of policies, standards, and guidelines that define action expectations to successfully implement the plan. The following terms clarify the level of commitment intended in the plan and reflect the expectation or desired outcome. The application of each term to a particular policy or action is a deliberate application of these definitions.

Shall—This type of policy will always be followed. Shall represents an absolute commitment to the guidance expressed in the policy. (Similar action words: require, enforce, must, ensure)

Should—This type of policy will be followed in most cases and exceptions or degrees of implementation are acceptable with valid reasons. (Similar action words: may)

Allow—Permit someone else's initiative and support it unless there is a very good reason not to. (Similar action words: permit)

Restrict—This type of policy sets specified limits within which action and/or implementation will occur. (Similar action words: control, limit, contain)

Prohibit—This type of policy requires steps to actively prevent a specified condition or decision from occurring. (Similar action words: forbid, ban)

Other terminology may appear in certain policy statements. These terms are to be interpreted according to their similarity to the appropriate term described above.

Relationship to Other Plans

Background—Internal Consistency between General Plan and Municipal Code

State law requires that a general plan and zoning be consistent. The Harvest Landing Specific Plan process includes an amendment to the City’s General Plan and Municipal Code to rectify inconsistencies between these two documents and bring the General Plan and zoning land use designations into conformance.

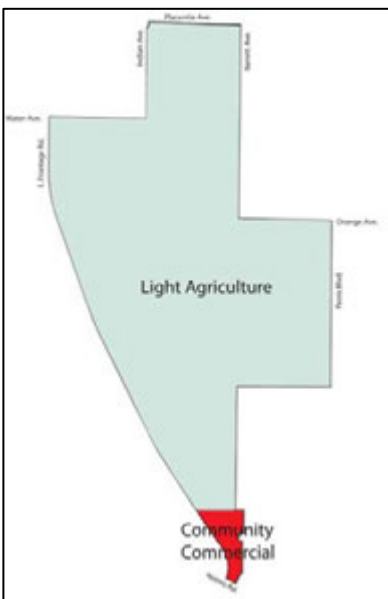
The City of Perris updated its General Plan in the spring of 2005. The new General Plan placed the Harvest Landing site within Planning Area 4, Freeway Business Park, of the General Plan and designated the project site for a combination of specific plan, business park, and community commercial land uses, as shown at left. The logic for these land use designations is clearly described in the General Plan: “The proximity of Planning Area 4 to I-215 makes it ideal for freeway-oriented land uses. New Business Park and freeway-oriented business can create additional employment opportunities, resulting in a larger customer base to support retail outlets, restaurants, and other service-oriented businesses that serve the before-during-after work needs of workers and professionals” (Perris General Plan 2005).

However, at the time of the writing of this Specific Plan, the new General Plan land use designations were inconsistent with the zoning designations for the site. As of 2005, the project site was zoned almost entirely Light Agriculture (A1) with a small portion on the southern tip zoned Community Commercial (CC), as shown at left.

The City intended to rectify the inconsistencies and bring the zoning map into conformance with the direction of the new General Plan. The Harvest Landing Specific Plan takes this step for the City, rectifies the General Plan and Zoning inconsistencies, and unifies the land use designations for the site.



General Plan designations as of April 26, 2005.



Zoning designations as of April 26, 2005.



Harvest Landing Specific Plan—Consistency with General Plan and Municipal Code

The Specific Plan implements the goals and policies of the General Plan (Appendix B, *General Plan Consistency Analysis*). The Harvest Landing Specific Plan also maintains the spirit and intentions of the newly adopted General Plan by utilizing the mixture of specific plan, business park, and community commercial land uses as called for in the new General Plan. However, the location and mixture of the General Plan land use designations are modified to reflect a unified land use design, viable mixture of uses, and market realities.

Through the Harvest Landing Specific Plan process, the General Plan and zoning are amended to present a consistent land use plan. This project includes: (1) the General Plan Amendment to change the existing Business Park and Community Commercial designations to Specific Plan, and (2) a zone change from the existing Light Agriculture to Specific Plan.

The Harvest Landing Specific Plan is adopted by City Council Ordinance and serves as the zoning for the project site. The Specific Plan provides the standards and development criteria to guide future development of the site. The text and diagrams of the Specific Plan address the planning of necessary infrastructure and facilities as well as land uses and open space.

Future subdivisions, building permits, and public works projects must be consistent with the Specific Plan (Government Code Sections 65455, 66473.5, 65860, and 65401). All projects that are found to be consistent with this specific plan will likewise be deemed consistent with the City's General Plan.

Relationship to the EIR

The California Environmental Quality Act (CEQA) was adopted to maintain the quality of California's environment. Its purpose is to inform decision makers, staff, and the public about the potential environmental impacts of development. Compliance with CEQA requires that a project be evaluated for potential impacts before being approved. Further, the state adopted a policy "that the public agencies should not approve projects as proposed if there are feasible alternatives or feasible mitigation measures available that would substantially lessen the significant environmental effects of such projects."



Revised General Plan land use designation of Specific Plan.



Revised zoning designation of Specific Plan.

Adoption of a specific plan is a project subject to CEQA. As such, an environmental impact report (EIR) has been prepared to analyze and disclose the significant environmental impacts of the Harvest Landing Specific Plan, as well as plan alternatives and the means by which possible environmental effects may be reduced or avoided. The EIR is incorporated into this Specific Plan by reference and is attached separately.

Summary of Opportunities and Constraints

The opportunities and constraints that exist on the Harvest Landing site are the keys to determining the design, layout, and appropriate uses for the development of the property. For example, the presence of I-215 dictates that noise-sensitive uses be located away from the freeway or modified to mitigate the noise. On the other hand, the freeway provides visibility for certain business uses that should be located adjacent to the freeway.

The following is a list of the opportunities and constraints that were present at the time of the writing of this document.

Opportunities

Interstate 215

Interstate 215 runs along the western edge of the project site. The freeway provides clear visibility of the site as well as direct access via Nuevo Road. The land adjacent to the freeway is an ideal location for business park, professional office, and retail uses, which will benefit from the proximity to this major transportation corridor. The placement of business uses along the freeway will serve as both a noise and visual buffer between the freeway and residential uses within the project area as well as provide for the creation of a significant number of jobs in the City.

Surrounding Uses

Harvest Landing is surrounded by several uses that influenced the design of the project. Critically, there are four significant commercial retail uses either adjacent or in close proximity to the site. These influenced the design and proposed uses in the Harvest Landing Specific Plan.



Immediately east of Harvest Landing is an approximately 49.3-acre area designated for commercial uses on the City's Zoning and General Plan maps. On July 10, 2007, a proposal to establish the Perris Marketplace, a 520,000 square foot commercial project on this site, was approved. The existing Perris Plaza with theaters, restaurants, and retail uses bounds the project on the south. In addition, the Perris Town Center is located to the south across Nuevo Road. There are several other smaller retail centers along Perris Boulevard. Given these uses and as detailed below, the convenience retail market is well served in the area. Therefore, the Harvest Landing Specific Plan is focused on providing residences to help bolster the commercial uses and providing locations for office, light industrial, and freeway-dependent retail uses.

The Val Verde Elementary School is adjacent to the northwestern corner of the site across Indian Avenue. The proximity of the Val Verde Elementary School dictates that compatible uses and uses that minimize impacts on the operation of the school should be located nearby.

Constraints

Future Business Market

Gregory Stoffel & Associates was commissioned to prepare a retail market analysis for the project (see Appendix C). According to the market study, population and demand within the site's market area is insufficient to justify additional retail uses/acreage along Perris Boulevard north of Nuevo Road. Given the proposed commercial project immediately east of the site with its Wal-Mart Supercenter, the existing Wal-Mart store on the northwest corner of Perris Boulevard and Orange Avenue may be vacated, adding to the supply of retail space in the area. The report adds that as more retail sites become available in the area, particularly those with freeway visibility and accessibility, interior sites like those along Perris Boulevard will become less desirable.

Gregory Stoffel & Associates utilized an analysis prepared for the City of Perris by the Buxton Company that evaluated the retail potential of three sites within the City: (1) Perris Boulevard and Orange Avenue; (2) Redlands Avenue and San Jacinto Avenue; and (3) Perris Boulevard and Citrus Avenue. The Buxton Company determined that the site located at the intersection of Redlands Avenue and San Jacinto Avenue has the best chance to attract quality retailers. This strengthens the conclusion by Stoffel &

Associates that retail uses along Perris Boulevard are not optimal for Harvest Landing.

The retail market analysis concludes that the City does not need additional retail space along Perris Boulevard, especially retail space that will likely not perform up to retail industry standards. Accordingly, Harvest Landing is designed to increase residential population, which will help strengthen existing commercial uses, and to focus complementary business, office, and retail uses along the freeway frontage.

Agricultural Reserve

As of the writing of this Specific Plan, sections of the project site were mapped as Prime Farmland and Farmland of Local Importance, and the site was subject to the Williamson Act.

The State Department of Conservation, Farmland Mapping and Monitoring Program designates the site as Prime Farmland, which is farmland with the best combination of physical and chemical features, able to sustain long-term agricultural production. While it is important to maintain Prime Farmland when possible, the project site's location makes it ideal for more urban uses. The City's General Plan land use designations for the project area reflect the City's desire to allow for the urban use of this land. This issue is addressed in depth in the Environmental Impact Report for the Harvest Landing Specific Plan.

The California Land Conservation Act of 1965 (commonly referred to as the Williamson Act) enables City and County governments to enter into contracts with private landowners to voluntarily restrict specific parcels of land to agricultural or related open space use. The purpose is to preserve agricultural and open space lands by discouraging premature and unnecessary conversion to urban uses. In return, landowners receive property tax assessments that are much lower than normal because they are based upon farming and open space uses as opposed to full market value. The vehicle for these agreements is a rolling-term 10-year contract (i.e., unless either party files a "notice of nonrenewal" the contract is automatically renewed annually). Prior to development of the site, a notice of nonrenewal will be submitted to the City and County.



March Air Reserve Base/March Global Port

Harvest Landing is located near the March Air Reserve Base/March Global Port and falls within its influence area. Specifically, the northern portion of the project site falls within Influence Area 2 while the southern section of the site falls under Influence Area 3. Influence Area 2 allows for agricultural, industrial, and commercial uses and limits residential development to one dwelling unit per 2.5 acres. Generally, there are no flights directly over the site and as a result, the influence boundaries are expected to change. As of the writing of this Specific Plan, the March Air Reserve Base/March Global Port was in the process of updating its influence boundaries. It is important to note that given the scale and density of existing developments surrounding this site and within Influence Area 2, it is clear that the density limit and use restrictions have not been strictly followed.

The requirements of both Influence Area 2 and Influence Area 3 necessitate that all properties obtain Avigation Easements, which grant authority to fly directly over property, and notify property owners or prospective purchasers of noise, vibration, and other impacts related to airport operations.

Noise

The site is located between major noise sources. Vehicular noise from I-215, Perris Boulevard, and Placentia Avenue are critical factors to consider in the location, buffering and/or mitigation of noise sensitive uses such as residential uses.

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Section 2:

DEVELOPMENT PLAN



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SECTION 2: DEVELOPMENT PLAN

This section describes the proposed development concept for Harvest Landing. The land use plan is shown on Figure 2-1, *Land Use Plan*, and the build-out potential is described in Table 2-1, *Statistical Data*, later in this section.

Introduction

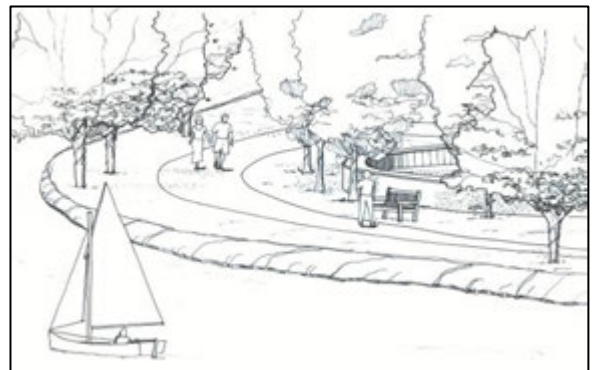
The Harvest Landing Specific Plan guides the development of a diverse mixture of uses all interconnected by a clear and unifying structure. Generous landscaping, wide paseos, extensive recreational amenities, and a unifying open space system provide a structure in which a wide variety of residential and business uses are arranged.

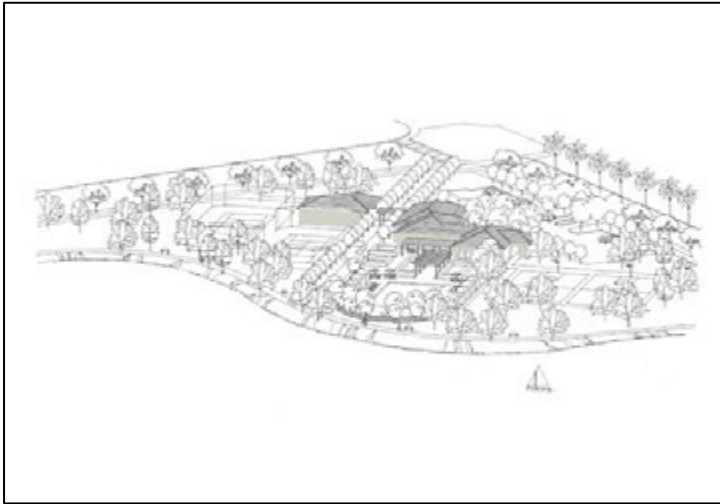
Heart and Soul

The heart and soul of Harvest Landing are its open space amenities. Key among those is a 19.4-acre recreational amenity consisting of Harvest Lake and Harvest Lake Park. This amenity is envisioned as a combination of open space and water feature around which a variety of residential product types and community activities are arranged. Harvest Landing is designed to provide views of Harvest Lake from the major entryways and to maximize residential units around its perimeter.

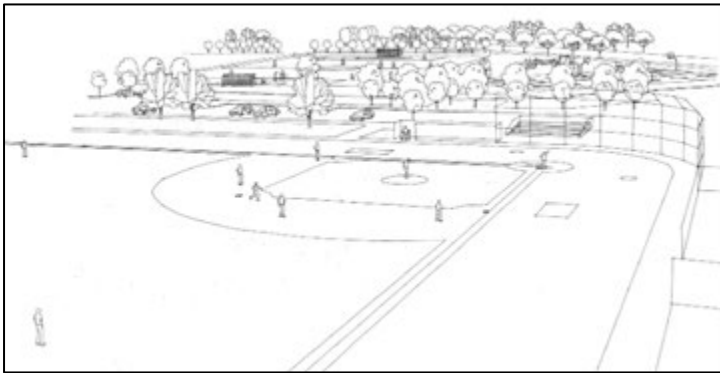


The approximately 11-acre Harvest Lake will have an undulating edge and meandering trail so residents and visitors can stroll around and enjoy this amenity. Harvest Lake is envisioned to be a recreational feature and is designed to accommodate nonmotorized water activities like fishing, paddle-boating, canoeing, and sailing. Harvest Lake is surrounded by a paseo connecting the recreation center on the northern shore with pocket parks on the eastern and western shores of the lake.

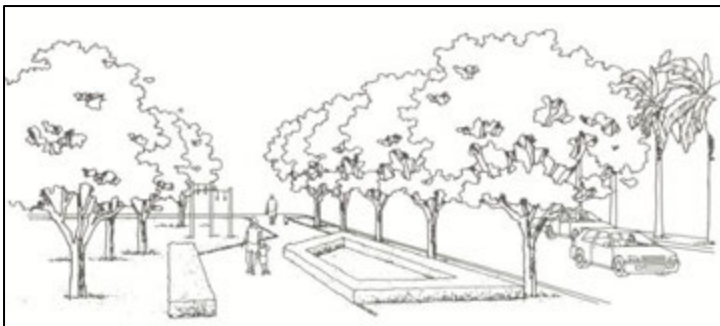




In combination with the lake, a 3.1-acre recreation center, which is located along Orange Avenue, will form the hub of activity for the community. This community center will provide various recreation opportunities and facilities, such as sports courts, a community pool, and a clubhouse.



Harvest Landing also features the approximately 17-acre Harvest Landing Sports Park, which is located north of Orange Avenue along Barrett Avenue. This park will provide for active recreation uses, such as soccer and baseball fields, sports courts, and tot lots, and serve a dual purpose for drainage detention.



A system of paseos is located throughout Harvest Landing to enhance recreational opportunities, provide an alternative and safe means of traveling through the site without the automobile, and promote a healthy lifestyle for the residents. Paseos are generally located around the lake, along Orange, Indian, and Avocado Avenues, and in the interior of the site. The paseos are intended for pedestrian circulation and to provide additional opportunities for active and passive recreation uses, such as jogger's exercise courses and picnic areas. The paseos also provide separation between neighborhoods, further adding to the unique identity of each neighborhood and serving as additional buffering from other uses, such as roadways and commercial areas.

Lifestyle Choices

Set among this rich variety of amenities and open space features is a wide variety of housing types and styles to accommodate numerous lifestyle choices. The array of residential products envisioned will serve a wide range of the residential market—families, single professionals, first-time homebuyers, those looking to move up, empty-nesters looking to downsize, and seniors.



Housing types range from single-family detached to small-lot detached, townhomes, apartments, courtyard clusters, and condominiums. In all, the plan accommodates 1,860 future residential units.

Flexible Business Options

Harvest Landing provides not only a rich variety of lifestyle choices, but also a wealth of flexible business and employment options. Harvest Landing accommodates slightly fewer than one job per residential unit. This will help the City as a whole to enhance its employment base and maintain its jobs-to-housing balance.

Given the wealth of existing and planned retail uses in the immediate area, Harvest Landing instead accommodates a unique blend of business uses that complement existing uses and broaden the City's job base. The approximately 81-acre Multiple Business Use area along East Frontage Road and I-215 will serve as a regional jobs center. This designation accommodates approximately 1.3 million square feet of business park, research and development, professional office, light industrial, and retail commercial uses.

Ideally situated to take advantage of the valuable freeway frontage, the Multiple Business Use designation is intended to be flexible to allow tenants to best meet market demand over time. This location also helps buffer residential uses to the east from the freeway noise.

A small commercial area in the southern portion of the project is adjacent to the existing Perris Plaza, located north of Nuevo Road. This commercial area allows for a wide range of commercial uses, including professional offices, retail, and restaurant uses that complement the existing Perris Plaza.

The system of paseos also connects the Multiple Business Use area with the residences and recreational features of the plan to form an integrated community.

Unique Identity

Harvest Landing will be an identifiable and unique community. Distinctive entryways at major intersections will announce arrival into the site. Wide landscaped medians and landscaping will decorate the streets. Consistent streetscape, consisting of signage, lighting, and walls along the major streets will unify the development.

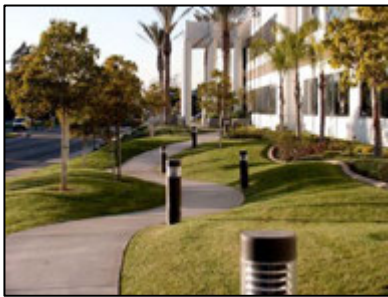


Examples of the types of residential products that are accommodated within the Plan.



A type of business use that could be located in the Multiple Business Use area.

Build-out Potential



Unified open spaces, landscaping, and amenities will help create the Plan's unique identity.

Table 2-1 provides a detailed breakdown of the build-out levels of Harvest Landing. Please note that the maximum number of dwelling units allowed within Harvest Landing is 1,860 units; however, the number of units within each planning area and land use category may vary per the provisions of Section 5, *Implementation*, which allows for the transfer of residential density. Therefore, Figure 2-1, *Land Use Plan*, depicts the intended development pattern and subsequent transfers of units may result in the shifting of the residential land use categories and/or residential units shown in each planning area.

In addition, please note that the units depicted on Figure 2-1 are used to determine total units instead of the maximum density allowed in each land use category. For instance, the Low Density Residential land use category allows a density of 7.0 units per acre, which if multiplied by the total acres of that land use category (60.7 acres) would result in 425 units. Instead, the Land Plan provides for 345 units in Low Density Residential land use category. This difference is due to the variation in product and market segmentation that is desired to make Harvest Landing a unique living environment.



**Table 2-1
Statistical Data**

<i>Land Use</i>	<i>Acres</i>	<i>Density/ FAR</i>	<i>Units</i>	<i>Sq. Ft.</i>	<i>Pop¹</i>	<i>Jobs²</i>
Residential Uses						
Existing Residential (Residential Overlay ³)	4.5	1 unit/lot	4	NA	15	NA
Low Density Residential (0.5-7.0 units per acre)	61.5	0.5-7.0	345	NA	1,287	NA
Medium Density Residential (7.1-12.0 units per acre)	40.1	7.1-12.0	372	NA	1,388	NA
Medium High Density Residential (12.1-15.0 units per acre)	18.0	12.1-15.0	250	NA	933	NA
High Density Residential (15.1-22.0 units per acre)	45.4	15.1-22.0	889	NA	3,316	NA
<i>Subtotal</i>	<i>169.5</i>	<i>NA</i>	<i>1,860</i>	<i>0</i>	<i>6,938</i>	<i>0</i>
Business Uses						
Multiple Business Use	80.9	.35	NA	1,233,401	NA	1,233
Commercial (Usable)	4.8	.35	NA	73,181	NA	146
Commercial (Unusable ⁴)	2.8	NA	NA	NA	NA	NA
<i>Subtotal</i>	<i>88.5</i>	<i>NA</i>	<i>0</i>	<i>1,306,582</i>	<i>0</i>	<i>1,380</i>
Recreation and Open Space Uses						
Parks	8.3	NA	NA	NA	NA	NA
Paseos	5.0	NA	NA	NA	NA	NA
Harvest Landing Sports Park	16.5	NA	NA	NA	NA	NA
Harvest Lake	11.1	NA	NA	NA	NA	NA
Community Recreation Center	3.1	NA	NA	NA	NA	NA
<i>Subtotal</i>	<i>44.0</i>	<i>NA</i>	<i>0</i>	<i>0</i>	<i>0</i>	<i>0</i>
Other						
Roads	34.1	NA	NA	NA	NA	NA
Drainage/Detention	4.9	NA	NA	NA	NA	NA
<i>Subtotal</i>	<i>39.0</i>	<i>NA</i>	<i>0</i>	<i>0</i>	<i>0</i>	<i>0</i>
Grand Total						
GRAND TOTAL	341.0	NA	1,860	1,306,582	6,938	1,380

Notes:

The transfer of residential units, as discussed in Section 5, Implementation, may result in the shifting of units between land use categories; however, the maximum of 1,860 units shall not be exceeded.

¹ Population is based upon 3.73 persons per unit (Dept. of Finance 2005).

² Job Assumption: 500 square feet per employee in Commercial and 1,000 square feet per employee in Multiple Business Use area. (Source: County of Riverside General Plan)

³ Since the underlying land use and overlay allow either business uses or residential uses, for purposes of buildout analysis it has been assumed that the four existing residences will remain.

⁴ 2.8 acres of the commercial parcel is developed as a parking lot in conjunction with the adjacent commercial center and will remain unchanged through this Specific Plan.

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LAND USE PLAN



Statistical Summary

LAND USE	ACRES	UNITS
Residential		
Existing	4.5	4
Low	61.5	345
Medium	40.1	372
Medium High	18.0	250
High	45.4	889
Multiple Business Use	80.9	---
Commercial	7.6	---
	(of which 2.8 AC is existing parking)	
Harvest Landing Sports Park	16.5	---
Community Recreation Center	3.1	---
Harvest Lake Park	8.3	---
Harvest Lake	11.1	---
Paseo	5.0	---
Drainage/Detention	4.9	---
Street ROW	34.1	---
TOTAL	341.0	1,860

Note: The maximum number of dwelling units allowed within Harvest Landing is 1,860 units; however, the number of units within each planning area and land use category may vary per the provisions of Section 5, Implementation, which allows for the transfer of residential density. Therefore, this land plan depicts the intended development pattern and subsequent transfers of units may result in the minor shifting of the residential land use categories and/or residential units shown in each planning area. The units depicted on this plan are used to determine total units not the maximum density of each land use category.



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Section 3:

DEVELOPMENT CRITERIA



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SECTION 3: DEVELOPMENT CRITERIA

This section includes land use designations, permitted uses, and development standards that are intended to shape the physical form of the project. In addition, it includes the mobility plan, parks and open space plan, and grading plan.

Land Use Designations and Permitted Uses

The Harvest Landing Specific Plan contains 13 different land use categories, including: 4 residential designations, 1 multiple business use classification, 1 commercial designation, 5 open space/recreation categories, and 1 overlay. Table 3-1, *Land Use Categories*, provides a description of the intended use of each land use category.

Table 3-2, *Permitted Uses*, lists specific land uses permitted within each land use category. For each land use designation, specific uses shall be allowed by right (permitted), by administrative review (administrative permit), conditionally upon the approval of a conditional use permit (conditionally permitted), or prohibited. The inclusion of any uses not specifically listed shall be subject to the discretion of the Director of Development Services and/or Planning Commission using the spirit of this Specific Plan as a guide.

**Table 3-1
Land Use Categories**

Land Use Category	Description of Category
Low Density Residential 0.5–7.0 units per acre	Provides for low density detached single-family residential uses.
Medium Density Residential 7.1–12.0 units per acre	Provides for small-lot single-family detached residential uses and medium density single-family attached dwellings (E.G. duplex, triplex, fourplex, cluster/row townhome).
Medium High Density Residential 12.1–15.0 units per acre	Provides for medium high density multifamily residential uses (E.G. fourplex, cluster/row townhome, and stacked flats).
High Density Residential 15.1–22.0 units per acre	Provides for high density multifamily residential uses (E.G. fourplex, cluster/row townhome, and stacked flats).
Multiple Business Use (MBU)	Provides for a mix of professional office, light industrial, research, and development, business park, retail commercial, related storage and support services uses, paseos, and pedestrian linkages.
Commercial	Provides for professional offices, retail uses, entertainment centers, and restaurants.
Paseo	Provides for a range of open space opportunities (excluding large sports fields and facilities) such as multipurpose trails, tot lots, picnic areas, and jogger’s exercise courses.
Harvest Lake Park and Harvest Landing Sports Park	Provide for active and passive open space and recreation opportunities, including sports parks, athletic fields, playgrounds, picnic areas, and neighborhood parks. Also allows for the detention of storm water.
Community Recreation Center (Comm. Rec.)	Provides for the creation of private community recreation facilities and related amenities.
Harvest Lake	Provides for water bodies and related uses, including boating facilities, piers, and docks for nonmotorized activities that may include canoeing, paddle-boating, fishing, and sailing. No swimming is allowed in the lake.
Drainage/Detention	Provides for drainage and detention uses, including drainage swales and detention basins.
Residential Overlay	In addition to the underlying land use designation, this overlay provides for one residential unit per existing lot.
Street Right-of-Way (ROW)	Accommodates backbone circulation system within the project.



**Table 3-2
Permitted Uses**

Legend: Permitted (P), Administrative Permit (A), Conditionally Permitted (C), Prohibited (X)

Use	Low Res.	Medium Res.	Medium High Res.	High Res.	Commercial	MBU	Paseo	Park and Sports Park	Comm. Rec.	Harvest Lake
Residential Uses										
Convalescent Facility ¹	C	C	C	C	X	X	X	X	X	X
Day Care Home, Large (7-12 children) ¹	C	C	C	C	X	X	X	X	X	X
Day Care Home, Small (0-6 children) ¹	P	P	P	P	C	C	X	X	X	X
Second Dwelling Unit	C	C	X	X	X	X	X	X	X	X
Multifamily Dwellings ¹	X	P	P	P	X	X	X	X	X	X
Residential Care Facility (1-12 persons) ¹	A	A	X	X	X	X	X	X	X	X
Single-Family Dwellings	P	P	X	X	X	X	X	X	X	X
Recreational Uses										
Boat House	X	X	X	X	X	X	C	C	P	P
Clubhouse	P	P	P	P	X	X	C	P	P	X
Dock, Pier	X	X	X	X	X	X	P	P	P	P
Open Spaces/Parks	P	P	P	P	P	P	P	P	P	P
Paseos (bicycles, pedestrian paths)	P	P	P	P	P	P	P	P	P	X
Swimming Pool/Spa	P	P	P	P	X	C	X	P	P	X
Tennis Court, Private or Public	P	P	P	P	X	C	X	P	P	X
Trails	P	P	P	P	P	P	P	P	P	X
Residential Accessory Uses ¹										
Antennae, Vertical/Satellite Dish	P	P	P	P	P	P	X	X	P	X
Patio/Gazebo	P	P	P	P	P	P	P	P	P	X
Storage Structures (less than or equal to 120 s.f.)	P	P	P	P	P	P	X	C	C	X
Commercial Recreational Vehicle and Boat Storage (no hookup)	X	X	X	X	X	C	X	X	X	X
Office Uses										
Administrative and Professional Offices/Services	X	X	X	X	P	P	X	X	X	X
Automotive Related Uses										
Auto Parts Sales	X	X	X	X	P	P	X	X	X	X
Auto/Marine Repair (e.g., bodywork, engine and drive train, painting, misc. work)	X	X	X	X	C	C	X	X	X	X
Car, RV, Truck, and Marine Sales	X	X	X	X	P	P	X	X	X	X
Car Washes	X	X	X	X	C	P	X	X	X	X
Service Stations ¹	X	X	X	X	C	C	X	X	X	X
Vehicle Leasing/Rental	X	X	X	X	C	C	X	X	X	X
Automotive Stereo Shops	X	X	X	X	P	P	X	X	X	X
SROs										
Single Room Occupancy (SROs)	X	X	C	C	X	C	X	X	X	X
Eating, Drinking Establishments										
Deli/Sandwich Shops/Donut Shops (less than 12 fixed seats)	X	X	X	X	P	P	X	X	P	X
Night Clubs/Bars/Lounges ¹	X	X	X	X	C	C	X	X	X	X
Restaurants, No Drive Thru ¹	X	X	X	X	P	P	X	X	P	X
Restaurants, With Drive Thru ¹	X	X	X	X	C	C	X	X	X	X
Entertainment										
Adult Entertainment	X	X	X	X	X	X	X	X	X	X
Auditoriums, Convention Halls, and Theaters	X	X	X	X	C	C	X	X	X	X
Video Arcades	X	X	X	X	C	C	X	X	X	X
Financial										
Commercial establishments that engage in monetary transactions not directly related to the sale of a product/service.	X	X	X	X	P	P	X	X	X	X
Check-Cashing Facilities	X	X	X	X	X	X	X	X	X	X
Medical										
Blood Banks	X	X	X	X	P	P	X	X	X	X
Health Clinics/Outpatient Surgery	X	X	X	X	C	C	X	X	X	X
Hospitals ¹	X	X	X	X	C	C	X	X	X	X
Medical Equipment and Supplies	X	X	X	X	P	P	X	X	X	X
Outpatient Treatment Programs	X	X	X	X	C	C	X	X	X	X
Senior/Congregate Care Facilities	X	X	C	C	C	C	X	X	X	X
Personal Services										
Barber/Beauty/Nail Shops	X	X	X	X	P	P	X	X	X	X
Dance Schools/Karate Studios	X	X	X	X	P	P	X	X	C	X
Dry Cleaners	X	X	X	X	P	P	X	X	X	X
Health/Athletic Clubs ¹	X	X	X	X	P	P	X	X	C	X
Laundromats (self-serve)	X	X	X	X	P	P	X	X	X	X

**Table 3-2
Permitted Uses**

Legend: Permitted (P), Administrative Permit (A), Conditionally Permitted (C), Prohibited (X)

<i>Use</i>	<i>Low Res.</i>	<i>Medium Res.</i>	<i>Medium High Res.</i>	<i>High Res.</i>	<i>Commercial</i>	<i>MBU</i>	<i>Paseo</i>	<i>Park and Sports Park</i>	<i>Comm. Rec.</i>	<i>Harvest Lake</i>
Business Uses										
Animal Boarding/Kennel (See Title 8 of the Municipal Code)	X	X	X	X	X	C	X	X	X	X
Bakeries (Commercial)	X	X	X	X	P	P	X	X	X	X
Catering Establishments	X	X	X	X	X	P	X	X	X	X
Cleaning/Janitorial	X	X	X	X	X	P	X	X	X	X
Convenience Stores	X	X	X	X	P	P	X	X	X	X
Copy Centers/Postal Service Centers/Blueprinting	X	X	X	X	P	P	X	X	X	X
Drug Stores ¹	X	X	X	X	P	P	X	X	X	X
Equipment Rental/Sales/Service Yard	X	X	X	X	X	C	X	X	X	X
Flower/Gift Shops	X	X	X	X	P	P	X	X	X	X
General Merchandise (including supermarkets)	X	X	X	X	P	P	X	X	X	X
Grocery Store ¹	X	X	X	X	P	P	X	X	X	X
Home Improvements with outdoor display of lumber, garden, and nursery items	X	X	X	X	P	P	X	X	X	X
Laboratories (e.g., film, medical, dental, and R&D, etc.)	X	X	X	X	X	P	X	X	X	X
Light Industrial & Light Manufacturing	X	X	X	X	P	P	X	X	X	X
Liquor Stores ¹	X	X	X	X	C	C	X	X	X	X
Mini-Malls (<25,000 sq. ft.)	X	X	X	X	C	X	X	X	X	X
Storage (indoor/outdoor)	X	X	X	X	X	C	X	X	X	X
Misc. Repairs/Service, indoor only	X	X	X	X	X	P	X	X	X	X
Nurseries/Garden Supplies	X	X	X	X	P	P	X	X	X	X
Office Equipment/Supplies	X	X	X	X	P	P	X	X	X	X
Pawn Shops /Thrift Shops	X	X	X	X	X	X	X	X	X	X
Professional Offices	X	X	X	X	P	P	X	X	X	X
Specialty Food Stores (no alcohol)	X	X	X	X	P	P	X	X	X	X
Smoke Shops	X	X	X	X	P	X	X	X	X	X
Swap Meets ¹	X	X	X	X	X	X	X	X	X	X
Welding and Related Uses	X	X	X	X	X	P	X	X	X	X
Warehousing and Distribution (<100,000 sq. ft.) ^{1&2}	X	X	X	X	X	P	X	X	X	X
Publishing/Printing Plants	X	X	X	X	X	P	X	X	X	X
Recycling Facilities ¹	X	X	X	X	X	C	X	X	X	X
Recycling Facilities, reverse vending only	X	X	X	X	A	A	X	X	X	X
Veterinary Services	X	X	X	X	P	P	X	X	X	X
Tattoo Parlors, Body-Piercing Studios	X	X	X	X	X	X	X	X	X	X
Other Uses										
Cable Companies	X	X	X	X	X	P	X	X	X	X
Child Care Center ¹	X	X	X	X	C	C	X	X	C	X
Churches ¹	C	C	C	C	X	C	X	X	X	X
Clubs, Lodges, and Meeting Halls ¹	X	X	X	X	C	C	X	X	P	X
Home Occupation	P	P	P	P	X	X	X	X	X	X
Drainage/Detention	P	P	P	P	P	P	P	P	P	P
Funeral Parlors/Mortuaries	X	X	X	X	X	X	X	X	X	X
Libraries	C	C	C	C	P	C	X	P	X	X
Museums	X	X	X	X	P	C	X	C	X	X
Radio/Television Broadcasting	X	X	X	X	X	C	X	X	X	X
Private/Public Utility Facilities	C	C	C	C	C	P	C	C	C	X
Private Schools (>12 students)	C	C	C	C	X	C	X	X	X	X
Social Service Uses/Centers	X	X	X	X	P	C	X	X	X	X
Vocational/Trade Schools	X	X	X	X	C	C	X	X	X	X
Wireless Communication Facilities ^{1&3}	X	X	X	X	C	C	X	X	C	X
Transportation/Distribution	X	X	X	X	X	C	X	X	X	X

Note:

The interpretation or inclusion of any uses not specifically listed in Table 3-2 shall be subject to the discretion of the Director of Development Services and/or Planning Commission upon appeal using this Specific Plan as a guide. See Section 5, *Implementation*, for the accommodation of minor amendments to this table.

¹ For definitions, see Chapter 19.08, Definitions, of the Municipal Zoning Code dated April 25, 2005.

² Facilities over 100,000 sq. ft. require a conditional use permit.

³ In the Community Recreation zone, wireless communication facilities must be concealed on or within the building and no poles are permitted.



Development Standards

Development standards are provided to regulate and shape the built form of Harvest Landing. Three types of development standards are included in this section: residential, business, and parks/open space. Within each of these three categories, there are two subcategories: (1) specific standards, which provide the tailored standards for each land use category and include such provisions as lot size, lot dimension, building height, and setback requirements; and (2) general development standards, which include regulations that apply to most, if not all, land use designations.

Specific development standards for each land use category are provided below. Tables 3-3 through 3-5 and Figures 3-1 through 3-6 provide standards for detached and attached residential uses. Table 3-6 addresses standards for the multiple business use and commercial land use categories. Finally, Table 3-7 presents standards for the four open space and recreation land use designations.

Residential Development Standards

Detached Residential Standards

Development standards for detached residential products, which are allowed in the Low, Medium, and Medium High land use categories, are provided on Table 3-3 and Figures 3-1 through 3-3.

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**Table 3-3
Development Standards – Detached Residential Uses**

<i>Standard</i>	<i>Low Density Residential</i>	<i>Medium Density Residential</i>	<i>Medium High Density Residential</i>
Maximum Density			
Residential density	0.5–7.0 dwelling units per acre	7.1 – 12.0 dwelling units per acre	12.1 – 15.0 dwelling units per acre
Building Site Specifications			
Minimum lot size per unit	3,500 s.f.	2,100 s.f.	1,800 s.f.
Minimum lot width ^{1 & 2}	40 feet	35 feet	30 feet
Minimum lot depth	70 feet	60 feet	50 feet
Corner lot streetside lot width	45 feet	40 feet	35 feet
Maximum lot coverage ³	50%	55%	75%
Front Setbacks			
To habitable structure ^{4, 12}	12 feet	10 feet	8 feet
To garage			
1. Front entry ⁵	20 feet	20 feet	Not allowed
2. Side entry	10 feet	8 feet	Not allowed
To unenclosed porch	10 feet	6 feet	6 feet
Side Setbacks from Street			
To structure ¹²	10 feet	10 feet	8 feet
To unenclosed porch	5 feet	5 feet	5 feet
Projections into setback ⁶	3 feet	3 feet	3 feet
Interior Side Setbacks			
To habitable structure ⁷	0 or 5 feet	0 or 5 feet	0 or 5 feet
To front-entry garage in rear 1/3 of lot	0 feet	0 feet	0 feet
To alley-entry garage	0 feet	0 feet	0 feet
Projections into setback ⁶	2 feet	2 feet	2 feet
Rear Setbacks			
To habitable structure	12 feet	8 feet	8 feet
To alley-entry garage	No required setback, but a 32-foot building-to-building separation must be maintained along alleys.		
To front-entry garage in rear 1/3 of lot	0 feet	0 feet	0 feet
Projections into setback ^{6, 8}	3 feet	3 feet	3 feet
Height			
Maximum height ⁹	35 feet	35 feet	35 feet
Other			
Building separation ¹⁰	10 feet	10 feet	10 feet
Minimum outdoor area ¹¹	250 s.f.	250 s.f.	250 s.f.

Notes:

All setbacks shall be measured from the property line to the structure. In the case of a detached condominium development, the building separations listed on Table 3-5 shall apply and the setbacks and building site specifications listed in this table shall not apply.

¹ Any lot with a width of 35 feet shall provide garage access from an alley.

² For knuckle and cul-de-sac parcels, the minimum parcel width shall be met 20 feet from the front property line.

³ The percentage of total site area covered by structures, open or enclosed, excluding the following uncovered structures: steps, courts, patios, terraces, walkways, and swimming pools.

⁴ For variation in the front setback, 33 percent of the units may encroach up to 3 feet into the front setback.

⁵ For front loaded garages only, a minimum of a 20-foot by 20-foot driveway apron is required.

⁶ Projections are uninhabitable architectural features that extend beyond the building face. Projections include features such as eaves, chimneys, bay windows, stairways, and other architectural detailing.

⁷ In the rear 1/3 of lots that employ a 0-foot setback on one side and a 10-foot setback on the other, the structure can encroach 5 feet into the interior side setback as long as a 15-foot rear building separation is maintained (See Figure 3-1).

⁸ Applies to habitable structure only. Garage projections are not permitted.

⁹ Maximum building height is defined as the height from the top of the finished grade to the top of the roof peak.

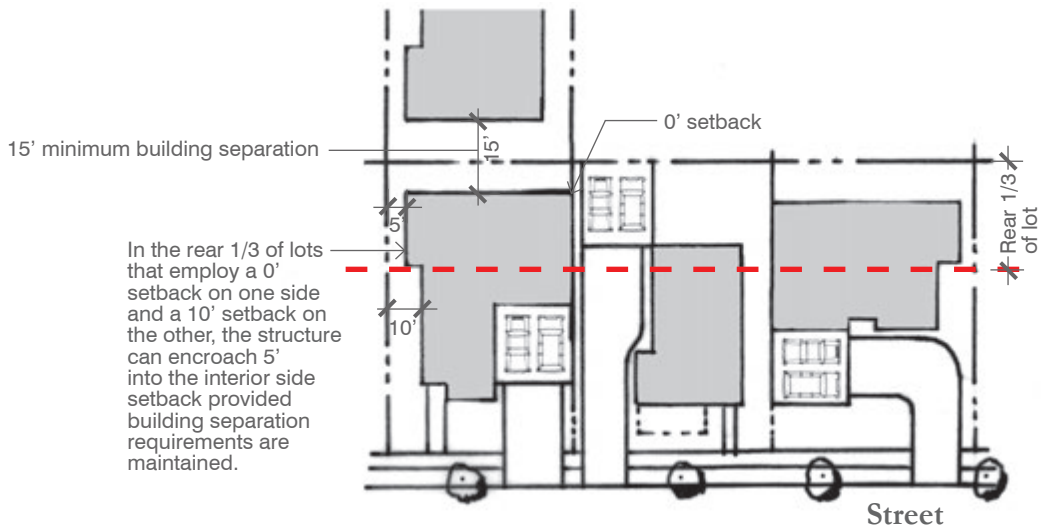
¹⁰ Zero lot line with a reciprocal easement excluded from building separation requirements.

¹¹ The outdoor area must consist of no less than 8 feet in any one direction.

¹² For freeway-adjacent units, a minimum of a 75 foot setback from the freeway right-of-way is required.

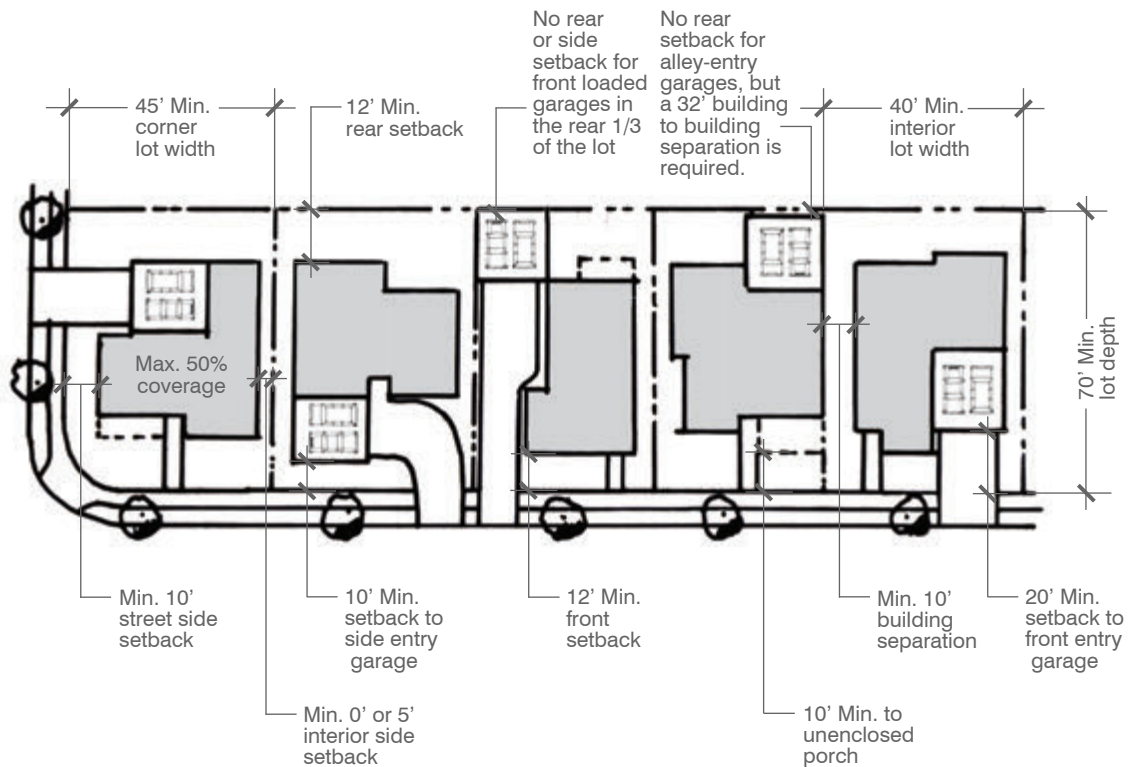
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SETBACK VARIATION (ILLUSTRATING NOTE 7 OF TABLE 3-3)



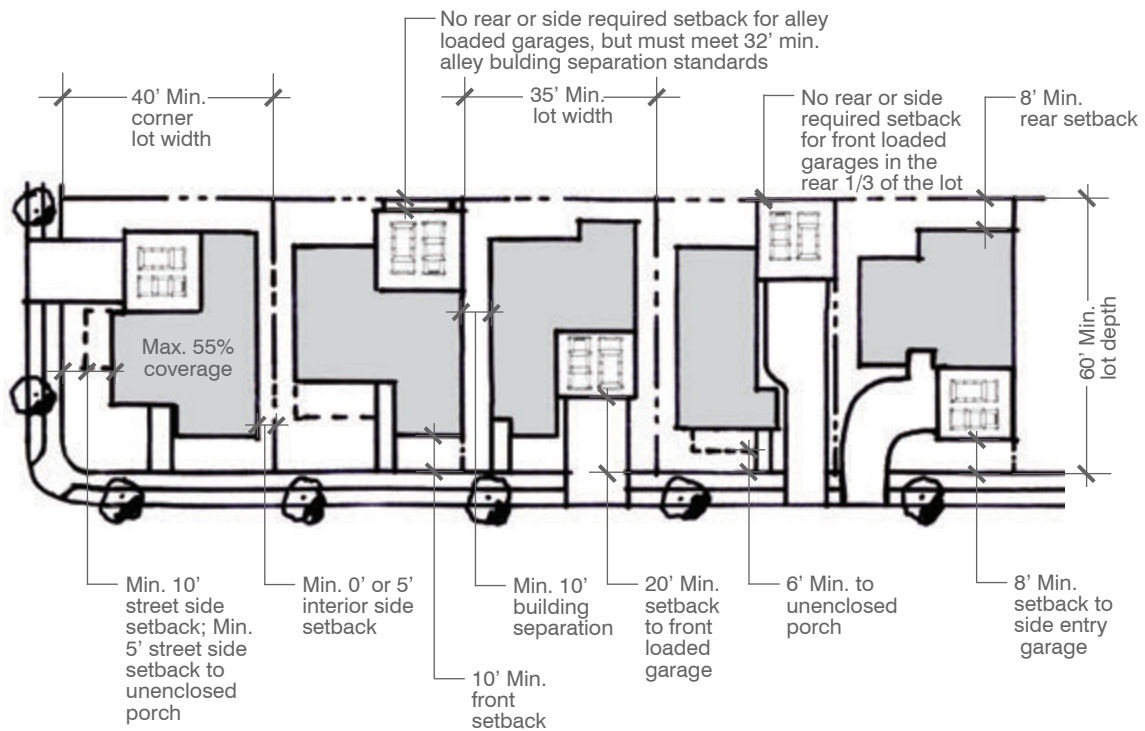
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LOW DENSITY RESIDENTIAL (DETACHED) DEVELOPMENT STANDARDS



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MEDIUM DENSITY RESIDENTIAL (DETACHED) DEVELOPMENT STANDARDS



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Attached Residential Standards

Development standards for attached residential products are provided on Tables 3-4 and 3-5 and Figures 3-4 through 3-6.

**Table 3-4
Development Standards – Attached Residential Uses**

Standard	Medium Density Residential	Medium High Density Residential	High Density Residential
Maximum Density			
Residential density	7.1-12.0 dwelling units per acre	12.1–15.0 dwelling units per acre	15.1–22.0 dwelling units per acre
Building Site Specifications			
Minimum parcel size	5,750 s.f.	0.5 AC including easements	1.0 AC including easements
Minimum parcel width ¹	50 feet	50 feet	60 feet
Minimum parcel depth	80 feet	80 feet	90 feet
Corner parcel streetside parcel width	60 feet	60 feet	70 feet
Maximum parcel coverage ²	55 %	55%	60 %
Front and Rear Setbacks			
To habitable structure ³	10 feet	Average of 20 feet ⁷	Average of 25 feet ⁷
To unenclosed porch	5 feet	5 feet ⁷	5 feet ⁷
Projections into setback ⁴	3 feet	3 feet	5 feet
Exterior Public Street Setbacks			
To habitable structure	10 feet	10 feet ⁷	10 feet ^{7 & 8}
To unenclosed porch	5 feet	5 feet ⁷	5 feet ⁷
Projections into setback ⁴	3 feet	3 feet	3 feet
Interior Setbacks			
None			
Height			
Maximum height ⁵	40 feet	40 feet	50 feet
Other			
Minimum usable common open space	NA	10% of parcel	15% of parcel
Minimum livable area	Studio – 550 s.f. 1 Bedroom – 700 s.f. 2 Bedroom – 900 s.f. 3 Bedroom – 1,100 s.f.		
Minimum usable private open space per unit ⁶	1 st Floor – 150 s.f. per unit 2 nd and 3 rd Floors – 75 s.f. per unit Minimum dimension of 6 feet in any direction.		

Notes:

All setbacks shall be measured from the greater of either (1) the property line or (2) from the back of the curb to the structure.

¹ For knuckle and cul-de-sac parcels, the minimum parcel width shall be met 20 feet from the front property line.

² The percentage of total site area covered by structures, open or enclosed, excluding the following uncovered structures: steps, courts, patios, terraces, walkways, and swimming pools.

³ Front entry tuck-under garages are permitted provided that they meet the setback requirement for the habitable structure and do not extend beyond the front wall of the structure.

⁴ Projections are uninhabitable architectural features that extend beyond the building face. Projections include features such as eaves, bay windows, stairways, porches, and other architectural detailing.

⁵ Maximum building height is defined as the height from grade to the top of the roof peak. An architectural projection such as a chimney or tower may exceed the maximum building height, but shall not exceed a height of 50 feet.

⁶ Private open space may include ground floor patios or courtyards, second- or third-floor balconies or decks, and rooftop decks. Private open space may be covered, but must be open on at least one side.

⁷ Setback requirements only apply to public streets that are exterior to the project.

⁸ For freeway-adjacent units, a minimum of a 75 foot setback from the freeway right-of-way is required.

**Table 3-5
Minimum Building Separation for Attached Residential Uses**

Building Orientation	1 to 1 and 1 to 2 Stories	1 to 3 or 4 and 2 to 2 Stories	2 to 3 or 4 Stories	3 to 3 or 4 and 4 to 4 Stories
Front to Front	15 feet	20 feet	25 feet	30 feet
Rear to Rear	10 feet	15 feet	20 feet	25 feet
Front to Rear	15 feet	15 feet	20 feet	20 feet
Side to Side	One half of height of taller building.			
Sidewalls Parallel w/Front or Rear Walls	15 feet	30 feet	30 feet	30 feet
Obliquely Aligned	Distances specified above may be decreased by 5 feet at one building corner provided there is an equal or greater distance increase at the other end.			
Projections into building separation area ¹	3 feet maximum			

Notes:

Building separations also apply to detached, condominium developments.

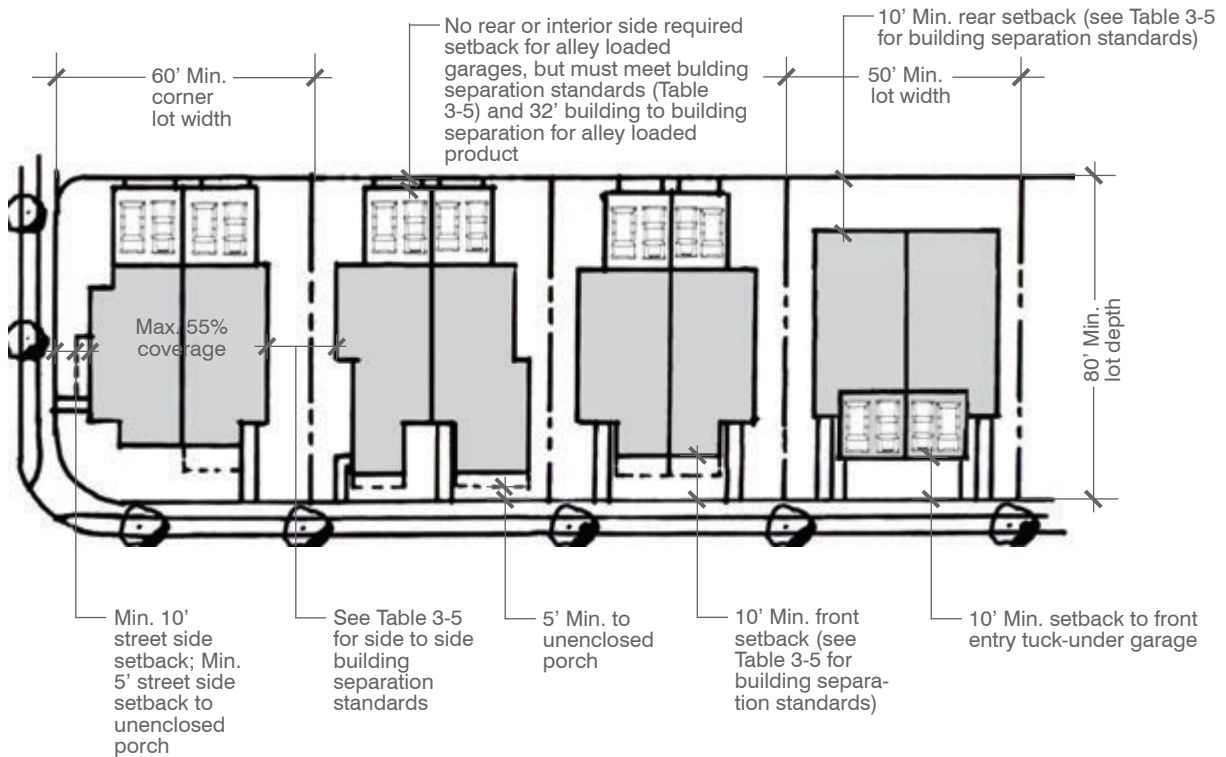
Zero-width reciprocal easements excluded from building separation requirements.

Building separations shall be measured from habitable structure to habitable structure.

¹ Projections are uninhabitable architectural features that extend beyond the building face. Projections include features such as eaves, bay windows, stairways, porches, and other architectural detailing.

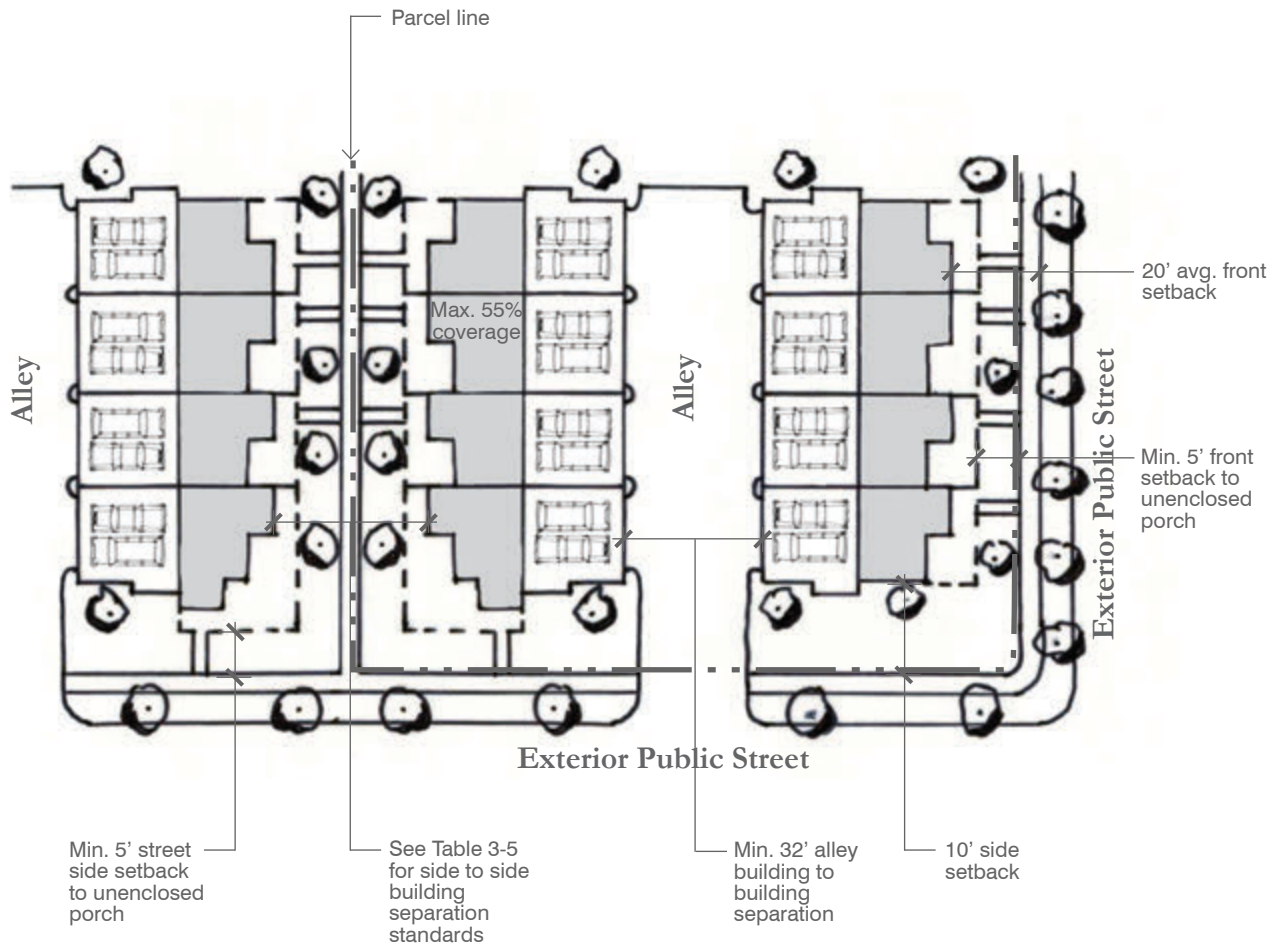
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MEDIUM DENSITY RESIDENTIAL (ATTACHED) DEVELOPMENT STANDARDS



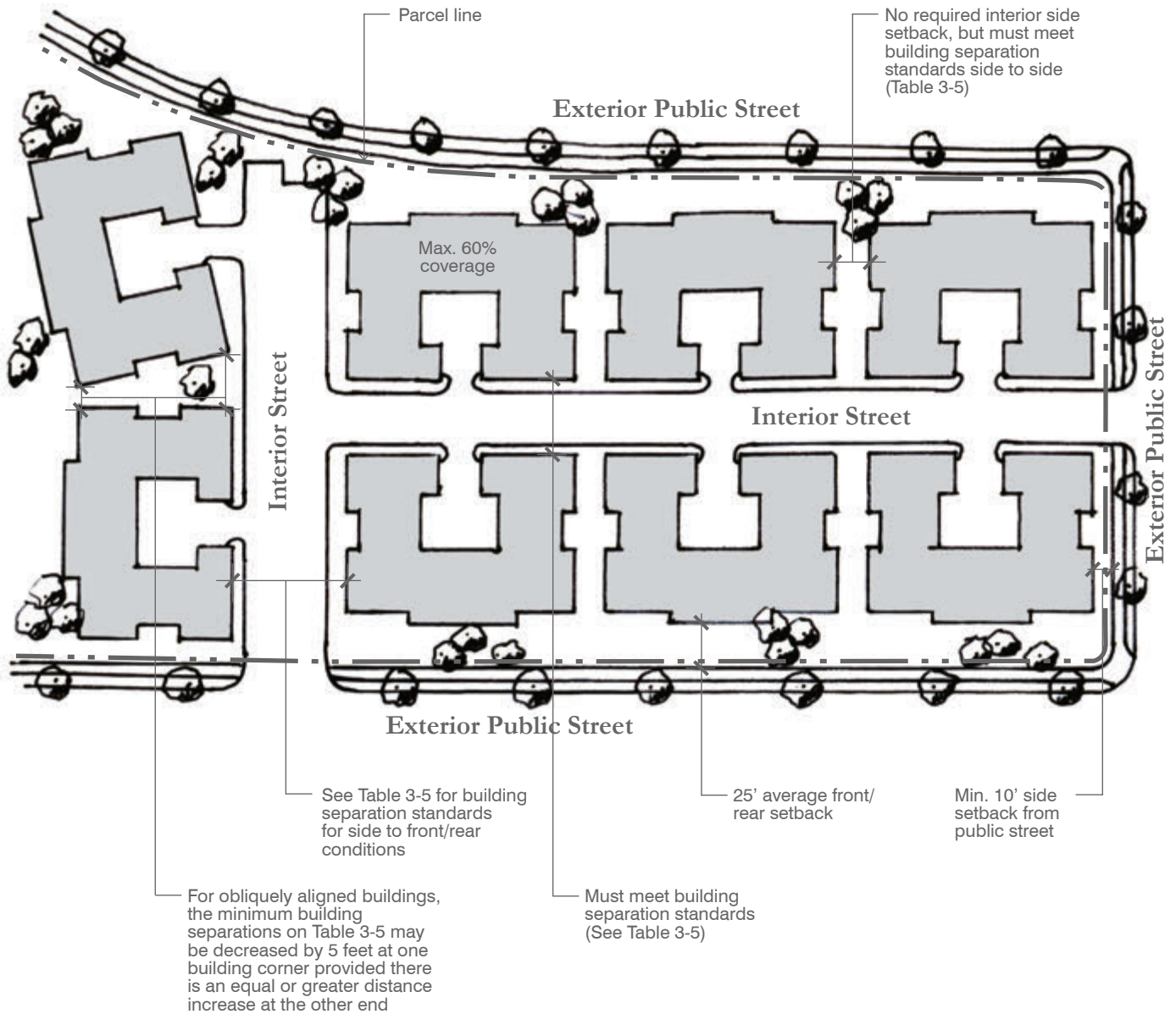
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MEDIUM HIGH RESIDENTIAL (ATTACHED) DEVELOPMENT STANDARDS



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**HIGH DENSITY RESIDENTIAL (ATTACHED)
DEVELOPMENT STANDARDS**



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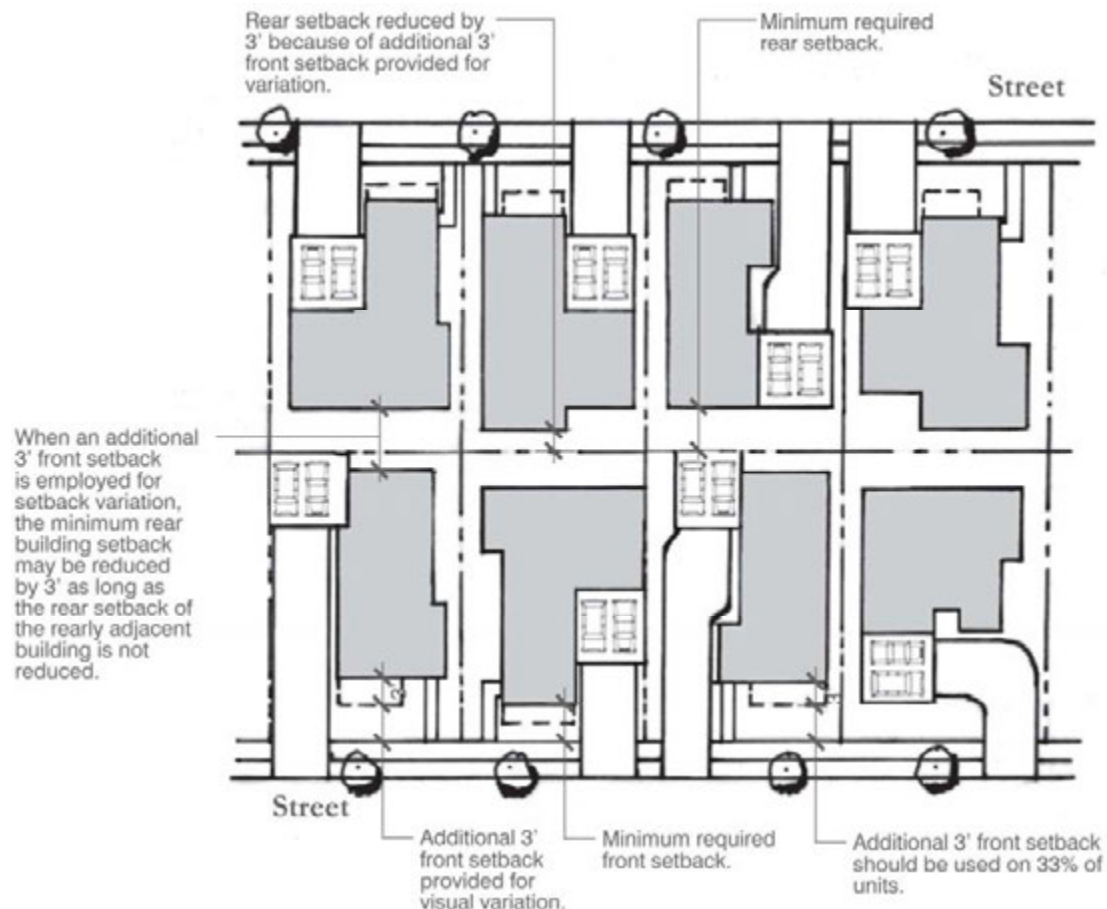
General Residential Development Standards

The following General Development Standards apply to all residential uses within Harvest Landing. Unless otherwise stated, they are consistent with the City of Perris Municipal Zoning Code dated April 25, 2005.

Product Variation

Harvest Landing will be an attractive and visually interesting development. Accordingly, residential neighborhoods will include a variety of product types and design styles as follows:

- All single-family detached projects shall include a variety of front yard building setbacks. One-third of the units should have different front yard setbacks by three feet. In these cases, the rear setbacks may be reduced by three feet.





Employ a variety of building materials and colors to add variety to a neighborhood.

- All single-family detached projects over five units should have a minimum of three different material and color palettes. No two homes with identical color or materials palettes shall be located adjacent to one another.
- All single-family detached projects over five units shall include a minimum of three unit designs. No two homes with identical designs shall be located adjacent to one another.
- All single-family detached projects over five units shall have a minimum of three primary roof materials and roof designs. No two homes with identical roof designs and materials shall be located adjacent to one another.
- All attached projects shall employ techniques listed in the Building-Level Design Guidelines section of this Specific Plan that address variation of massing, setbacks, rooflines, materials, colors, and styles, and address corner treatments, building/entry orientation, and building/façade articulation.

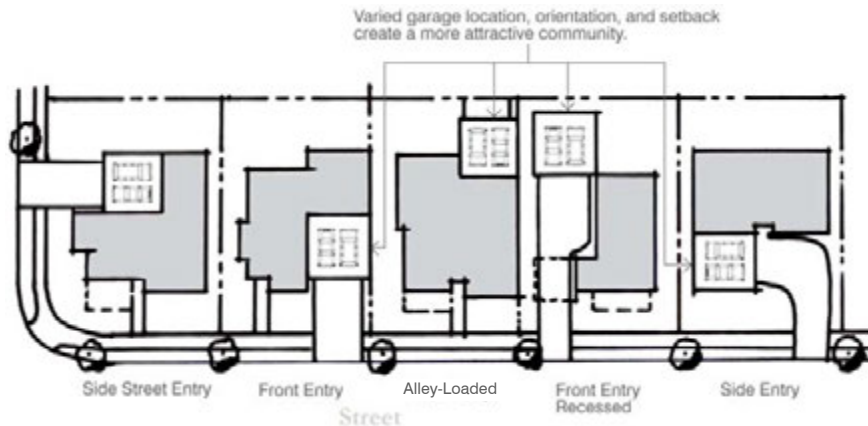
Garage Variation

To avoid the monotony of single-family neighborhoods that employ the same garage placement, projects in Harvest Landing are required to include a variety of garage placements and orientations. Standard garage placement in detached projects is a front-loaded garage set back 20 feet from the front property line. Single-family detached projects with more than five units shall utilize alternative garage orientation and placement on 50 percent of the units. The following are potential alternative garage placements:

- Alley-loaded garages
- Side-entry garages
- Split garages
- Courtyards or driveways providing multiple unit access
- Straight-in garages in rear two-thirds of the lot



For all garage orientations, roll-up garage doors with automatic openers are required.



Trash Receptacles

Trash receptacles shall be completely screened from public rights-of-way, common areas, and parking areas, through site orientation, screening materials (walls/landscaping), or a combination of both. Private trash cans may be stored in garages or outdoors, but need a dedicated area of 3 feet by 9 feet.

Walls and Fences

Per Section 19.02.040.A, Restrictions for Walls, Fences, and Hedges, of the Municipal Zoning Code dated April 25, 2005.

Corner Clearance

Per Section 19.02.050, Corner Clearance, of the Municipal Zoning Code dated April 25, 2005.

Open Space Areas and Encroachments

Per Section 19.02.060, Open Space Areas and Encroachments, of the Municipal Zoning Code dated April 25, 2005.

Screening

Per Section 19.02.070, Screening, of the Municipal Zoning Code dated April 25, 2005.

Property Maintenance

Per Section 19.02.080, Property Maintenance, of the Municipal Zoning Code dated April 25, 2005.

Utilities

Per Section 19.02.100, Utilities, of the Municipal Zoning Code dated April 25, 2005.

Landscaping

Per Section 19.02.130.A, Landscaping, and Chapter 19.70, Landscaping, of the Municipal Zoning Code dated April 25, 2005, plus the following sections:

- Street tree requirements of section 19.70.050A
- Median requirements of section 19.70.050B, except shall also include Orange Avenue, Indian Avenue, and all project locals (see Figure 3-7, *Vehicular Circulation Plan*, for the location of project locals)
- Screening requirements of section 19.70.050H
- Water conservation requirements of section 19.70.020
- Residential subdivision requirements of sections 19.70.050C and 19.70.050D and shall adhere to the landscape guidelines found in this Specific Plan

Home Occupations

Per Section 19.02.140, Home Occupations, of the Municipal Zoning Code dated April 25, 2005.

Low Income and Senior Housing Bonuses

Per Chapter 19.57, Density Bonuses and Other Incentives for Low Income and Senior Housing, of the Municipal Zoning Code dated April 25, 2005.

Temporary Activities and Uses

Per Chapter 19.60, Temporary Activities and Uses, of the Municipal Zoning Code dated April 25, 2005.

Parking and Loading Standards

Per Chapter 19.69, Parking and Loading Standards, of the Municipal Zoning Code dated April 25, 2005, with the following exceptions:



Number of Spaces

- Two enclosed garage spaces are required for each detached unit in the Low Density Residential and Medium Density Residential land use designations.
- For attached units in the Medium Density Residential, Medium High Density Residential, and High Density Residential land use designations, the following shall apply:
 - One space per unit shall be within a carport or an enclosed garage.
 - Studio: 1 space/unit and 0.25 guest space/unit
 - One Bedroom: 1.5 spaces/unit and 0.5 guest space/unit.
 - Two Bedroom: 2 spaces/unit and 0.5 guest space/unit
 - Each additional bedroom: 0.5 space/unit up to 10 spaces, and 0.25 space/unit exceeding 10 spaces shall be distributed throughout development.

Signs

Per Chapter 19.75, Sign Regulations, of the Municipal Zoning Code dated April 25, 2005.

Second Dwelling Units

Per Chapter 19.81, Second Dwelling Units, of the Municipal Zoning Code dated April 25, 2005.

Child Care Facilities

Per Chapter 19.83, Child Care Facilities, of the Municipal Zoning Code dated April 25, 2005.

Residential Care Facilities

Per Chapter 19.84, Residential Care Facilities, of the Municipal Zoning Code dated April 25, 2005.

Wireless Telecommunication Facilities

Per Chapter 19.85, Wireless Telecommunication Facilities, of the Municipal Zoning Code dated April 25, 2005.

Detention/Drainage

Detention and drainage areas shall be permitted in all land use designations as necessary and on a case-by-case basis. When possible, these areas should be designed to blend in with the



surrounding development, be landscaped, and be designed to accommodate uses that can be flooded, such as active/passive recreation and natural open space.

Freeway Residential Edge Conditions

Freeway adjacent residential units shall be set back at least 75 feet from the freeway right-of-way. These units shall utilize noise reduction design components as necessary. See the provisions for Freeway Residential Edge Conditions contained in Section 4, *Design Guidelines*, for more information on the treatment of freeway adjacent residential uses.



Business-Related Standards

Development standards for developments in the Multiple Business Use and Commercial land use designations are provided on Table 3-6.

<i>Standard</i>	<i>Multiple Business Use</i>	<i>Commercial</i>
Maximum Intensity		
Maximum Nonresidential Intensity	0.35 FAR ¹	0.35 FAR ¹
Building Site Specifications		
Minimum Lot Size	15,000 s.f.	10,000 s.f.
Minimum Lot Width ⁵	100 feet	100 feet
Minimum Lot Depth	150 feet	100 feet
Maximum Lot Coverage	50%	50%
Front and Side Street Setbacks ²		
To Private Streets	10 feet	5 feet
To Local and Collector Streets	10 feet	5 feet
To Primary, Secondary, and Project Arterials	15 feet	10 feet
Interior Side Setbacks		
If Adjacent to Similar Uses	0 feet	0 feet
If Adjacent to Residential Uses ³	50 feet	50 feet
Rear Setbacks		
If Adjacent to Similar Uses	15 feet	15 feet
If Adjacent to Residential Uses ³	50 feet	50 feet
Height		
Maximum Height ⁴	60 feet	40 feet
Maximum Stories	4	2
Other		
Building Separation	0 feet or 10 feet	0 feet or 10 feet
Minimum Lot Frontage	100 feet	100 feet

Notes:

All setbacks shall be measured from the property line to the structure.

¹ An FAR of 0.5 is permitted for Class A Offices, as defined by the City and with City approval.

² For buildings over 25 feet in height, an additional 5 feet of setback shall be provided for each additional 10 feet of building height.

³ If loading and unloading areas are provided, then the setback shall be at least 100 feet to the loading bay doors.

⁴ Maximum building height is defined as the height from grade to the top of the roof pitch or top of parapet.

⁵ For knuckle and cul-de-sac parcels, the minimum parcel width shall be met 30 feet from the front property line.

General Business Development Standards

The following General Development Standards apply to all business-related (Commercial and Multiple Business Use areas) uses within the Harvest Landing Specific Plan. Unless otherwise stated, they are consistent with the City of Perris Municipal Zoning Code dated April 25, 2005.

Trash Receptacles

Trash receptacles shall be completely screened from public rights-of-way and parking areas, through site orientation, screening materials (walls/landscaping), or a combination of both.

Walls and Fences

Per Section 19.02.040.B, Restrictions for Walls, Fences, and Hedges, of the Municipal Zoning Code dated April 25, 2005.

Screening

Per Section 19.02.070, Screening, of the Municipal Zoning Code dated April 25, 2005.

Property Maintenance

Per Section 19.02.080, Property Maintenance, of the Municipal Zoning Code dated April 25, 2005.

Utilities

Per Section 19.02.100, Utilities, of the Municipal Zoning Code dated April 25, 2005.

Lighting

Per Section 19.02.110, Lighting, of the Municipal Zoning Code.

Landscaping

Per Section 19.02.130.B, Landscaping, and Chapter 19.70, Landscaping, of the Municipal Zoning Code dated April 25, 2005, plus the following sections:

- Street tree requirements of section 19.70.050A
- Median requirements of section 19.70.050B, except shall also include Orange Avenue, Indian Avenue, and all project locals. (See Figure 3-7, *Vehicular Circulation Plan*, for the location of project locals.)
- Screening of section 19.70.050H



- Water conservation requirements of section 19.70.020
- Carefully select plant species and locate landscaping to allow visibility of businesses and signs with freeway exposure.
- Commercial landscape requirements of section 19.70.050E with the exception of a minimum of one tree per six parking stalls. In addition, landscaping shall adhere to the landscape guidelines found in this Specific Plan.

Temporary Activities and Uses

Per Chapter 19.60, Temporary Activities and Uses, of the Municipal Zoning Code dated April 25, 2005.

Parking and Loading Standards

Per Chapter 19.69, Parking and Loading Standards, of the Municipal Zoning Code dated April 25, 2005.

Signs

Per Chapter 19.75, Sign Regulations, of the Municipal Zoning Code dated April 25, 2005.

Wireless Telecommunication Facilities

Per Chapter 19.85, Wireless Telecommunication Facilities, of the Municipal Zoning Code dated April 25, 2005.

Child Care Facilities

Per Chapter 19.83, Child Care Facilities, of the Municipal Zoning Code dated April 25, 2005.

Detention/Drainage

Detention and drainage areas shall be permitted in all land use designations as necessary and on a case-by-case basis. When possible, these areas should be designed to blend in with the surrounding development, be landscaped, and be designed to accommodate uses that can be flooded, such as active/passive recreation and natural open space.

Hazardous Materials

The Hazardous Materials Management Division (HMMD) of the Riverside County Environmental Health Department is responsible for regulating hazardous materials business plans, chemical inventory, hazardous waste, underground storage tanks, and risk

management plans in the County. The goal of the HMMD is to protect human health and the environment by ensuring that hazardous materials, hazardous waste, and underground storage tanks are properly managed. To accomplish this goal, the HMMD has several programs working with the regulated community and the public.

- **Business Emergency Plan/Handler Program.** Regulates facilities that handle and store on-site specified types and quantities of hazardous and acutely/extremely hazardous materials through permitting, routine facility inspections and development of detailed site plans indicating where hazardous materials are stored.
- **Hazardous Waste Generator.** Regulates facilities in the community that generate a hazardous waste. Regulatory requirements are fulfilled through a program of annual permits, supported by routine inspections.
- **Certified Unified Program Agency.** Conducts inspections of businesses that handle hazardous materials, generate hazardous waste, treat hazardous waste and/or maintain underground storage tanks.
- **California Accidental Release Prevention.** Works with local businesses to reduce the likelihood and severity of consequences of releases from hazardous materials.

Uses within Harvest Landing that propose to utilize chemicals or perform uses that fall under the purview of the HMMD shall be reviewed and approved prior to the issuance of the certificate of occupancy.

In addition, the Val Verde Elementary School is located across Indian Avenue from the northwestern corner of Harvest Landing. The proximity of the Val Verde Elementary School necessitates restrictions to the types of chemicals and emissions that can occur nearby. Accordingly, businesses that use chemicals or conduct uses are subject to regulation by the HMMD.



Parks and Open Space Standards

Development standards for developments in the open space and recreational land use designations are provided on Table 3-7.

Table 3-7
Development Standards – Open Space and Recreation Uses

<i>Standard</i>	<i>Paseo</i>	<i>Park and Harvest Landing Sports Park</i>	<i>Community Recreation Center</i>	<i>Harvest Lake</i>
Maximum Intensity				
Maximum Intensity	NA	0.025 FAR	0.5 FAR	NA
Building Setbacks¹				
From any public street	NA	10 feet	20 feet	NA
From any abutting residential building	25 feet	NA	NA	NA
From the lake	NA	0 feet	20 feet ²	NA
Building to building separation	NA	10 feet for one-story buildings; 15 feet for two-story buildings	10 feet for one-story buildings; 15 feet for two-story buildings	NA
Height				
Maximum height	25 feet	25 feet ³	35 feet ³	NA
Maximum stories	1	1	2	NA

Notes:

All setbacks shall be measured from the property line to the structure.

¹ Does not apply to shade structures of 150 s.f. or less and covered informational signs.

² Boathouses/water recreation facilities and attached docks and piers excluded.

³ Uninhabitable architectural projections, such as towers, turrets, and chimneys may exceed the maximum building height, but shall not exceed a height of 40 feet.

General Parks and Open Space Development Standards

The following General Development Standards apply to all developments in the parks and recreational areas (Paseo, Harvest Lake Park, Harvest Landing Sports Park, Community Recreation Center, and Harvest Lake designations) within the Harvest Landing Specific Plan. Unless otherwise stated, they are consistent with the City of Perris Municipal Zoning Code dated April 25, 2005.

Trash Receptacles

Trash receptacles shall be designed to complement the surrounding character or be completely screened from public rights-of-way and parking areas, through site orientation, screening materials (walls/landscaping), or a combination of both.

Walls and Fences

Per Section 19.02.040.B, Restrictions for Walls, Fences, and Hedges, of the Municipal Zoning Code dated April 25, 2005.

Screening

Per Section 19.02.070, Screening, of the Municipal Zoning Code dated April 25, 2005.

Utilities

Per Section 19.02.100, Utilities, of the Municipal Zoning Code dated April 25, 2005.

Lighting

Per Section 19.02.110, Lighting, of the Municipal Zoning Code dated April 25, 2005.

Landscaping

Per Section 19.02.130.B, Landscaping, and Chapter 19.70, Landscaping, of the Municipal Zoning Code dated April 25, 2005, plus the following sections:

- Street tree requirements of section 19.70.050A
- Median requirements of section 19.70.050B, except shall also include Orange Avenue, Indian Avenue, and all project locals (See Figure 3-7, *Vehicular Circulation Plan*, for the location of project locals.)



- Screening of section 19.70.050H
- Water conservation requirements of section 19.70.020

In addition, landscaping shall adhere to the landscape guidelines found in this Specific Plan.

Temporary Activities and Uses

Per Chapter 19.60, Temporary Activities and Uses, of the Municipal Zoning Code dated April 25, 2005.

Parking and Loading Standards

Per Chapter 19.69, Parking and Loading Standards, of the Municipal Zoning Code dated April 25, 2005.

Signs

Per Chapter 19.75, Sign Regulations, of the Municipal Zoning Code dated April 25, 2005.

Detention/Drainage

Detention and drainage areas shall be permitted in all land use designations as necessary and on a case-by-case basis. When possible, these areas should be designed to blend in with the surrounding development, be landscaped, and be designed to accommodate uses that can be flooded, such as active/passive recreation and natural open space.

Lake Adjacent Uses

Uses adjacent to the lake shall be designed to ensure the safety of users. Tot lots, outdoor play areas, and other children-oriented uses located adjacent to the lake shall include design features, perimeter landscaping, or low walls/fences to promote the safety of children utilizing these amenities.

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Mobility Plan

Harvest Landing is not just about the automobile, it is about providing real mobility options to allow convenient access to the region and promote a healthy lifestyle within the site.

The Harvest Landing Specific Plan ensures efficient circulation both within the project and between the project and adjacent areas through a comprehensive circulation plan. A network of roadways, multiple-use trails, and paseos throughout the project encourages a wide range of mobility options for the project's residents and visitors, including vehicular, pedestrian, bicycle, and transit opportunities.

Vehicular Circulation

Harvest Landing is well positioned to take advantage of existing regional transportation corridors. I-215 runs along the western edge of the project, providing access to Riverside and other areas within southern California and the nation. An existing interchange just south of the project at Nuevo Road provides almost direct access from the project to the freeway.

As shown in Figures 3-7A and 3-7B, *Vehicular Circulation Plan*, the Harvest Landing Specific Plan includes a vehicular circulation plan that fits into and complements the existing surrounding roadway system. (See Appendix K of the Harvest Landing EIR for more detail on the project's circulation system and mapping of proposed off site improvements.) Similar to the City's surrounding circulation system, the project's roadway system consists of a hierarchy of streets including arterials, secondary arterials, project arterials, collectors, local, and private streets and alleys.

All streets within Harvest Landing meet or exceed the City's General Plan future right-of-way standards for each roadway. Within the right-of-way, there is a slight variation from City standards in pavement area and sidewalk and parkway width to create a more pedestrian-friendly environment. The typical street sections are found on Figures 3-7A and 3-7B and described below.

***Perris Boulevard and
Placentia Avenue***

Perris Boulevard and Placentia Avenue bound the project on the east and north, respectively, providing excellent access to both local and regional destinations. Where these arterials run adjacent to the Specific Plan area, they have rights-of-way of 128 feet. These roadways will both have 14-foot-wide curbed central medians dividing six lanes of traffic. The arterials will be lined with 17-foot-wide parkways, providing pedestrians and cyclists with a safe and enjoyable means of movement along the arterials and helping to buffer adjacent uses from roadway noise impacts.

Primary Arterials (Sections A–B)

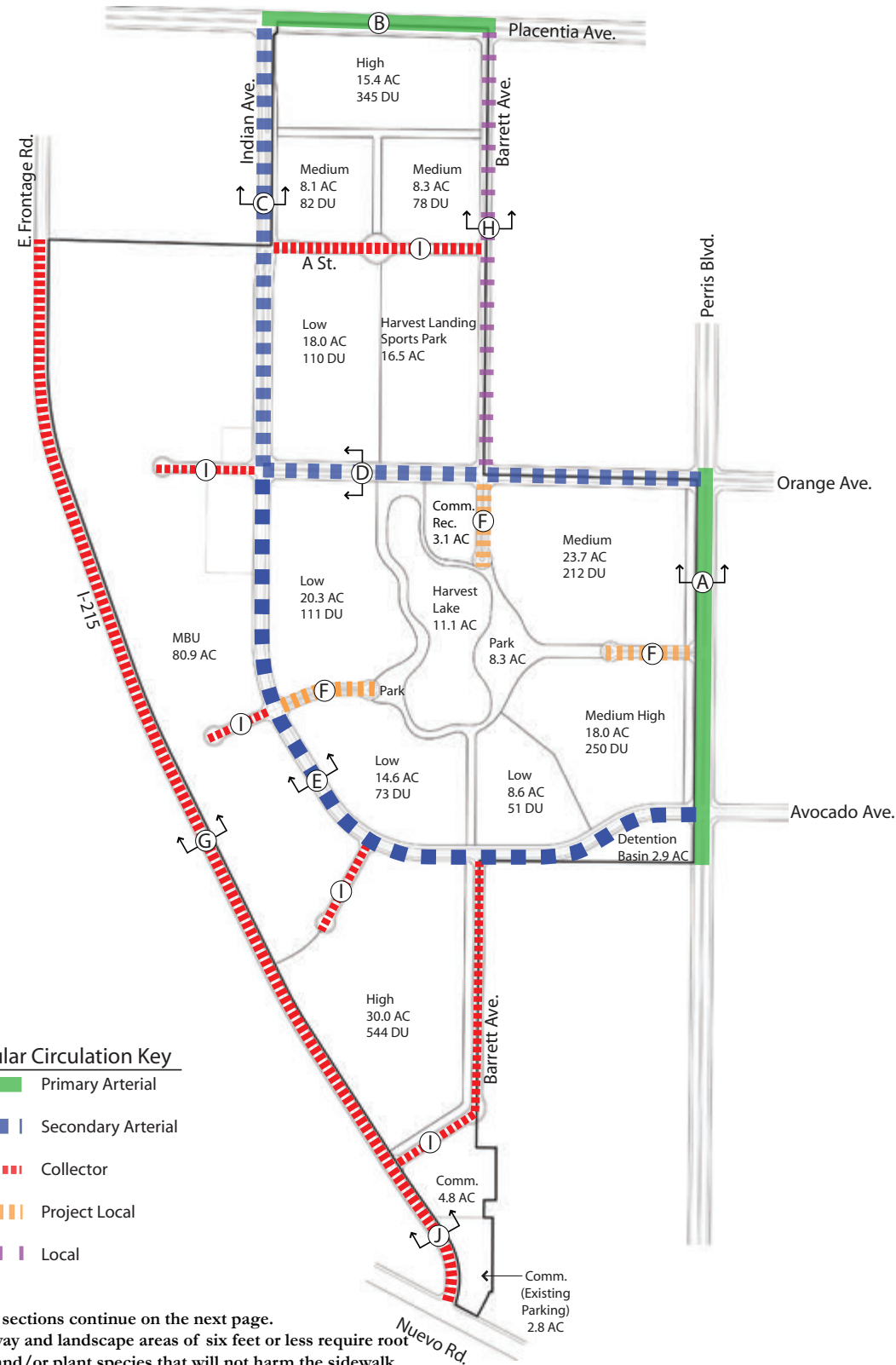
Primary Arterial roadways are intended to carry large volumes of relatively high-speed traffic between the region, different parts of the City, and Harvest Landing. Perris Boulevard and Placentia Avenue are the two arterials in the Harvest Landing Specific Plan and are further described below.

Perris Boulevard

Perris Boulevard serves as a main corridor within the City. It runs north-south along the eastern edge of the project, providing direct access between the project and the downtown area. Perris Boulevard is currently four lanes but is planned for expansion to six lanes (128-foot-wide right-of-way) by 2030. The 17-foot-wide western parkway combined with a 50-foot-wide drainage swale provides a 67-foot-wide buffer between the road and adjacent residential uses. See Figure 3-8 for a conceptual section and site plan of Perris Boulevard.

Placentia Avenue

Placentia Avenue runs east-west along the northernmost boundary of the site. As of the writing of this Specific Plan, this roadway had only two lanes and was incomplete between East Frontage Road and Indian Avenue. A planned interchange at Placentia Avenue and I-215, combined with the planned completion and expansion of Placentia Avenue to a six-lane, 128-foot-wide right-of-way, will provide improved access from the northern portion of the project site to surrounding areas and I-215. A 6-foot-wide sidewalk along the southern parkway will improve pedestrian and bicycle circulation along the northern edge of the Specific Plan area.



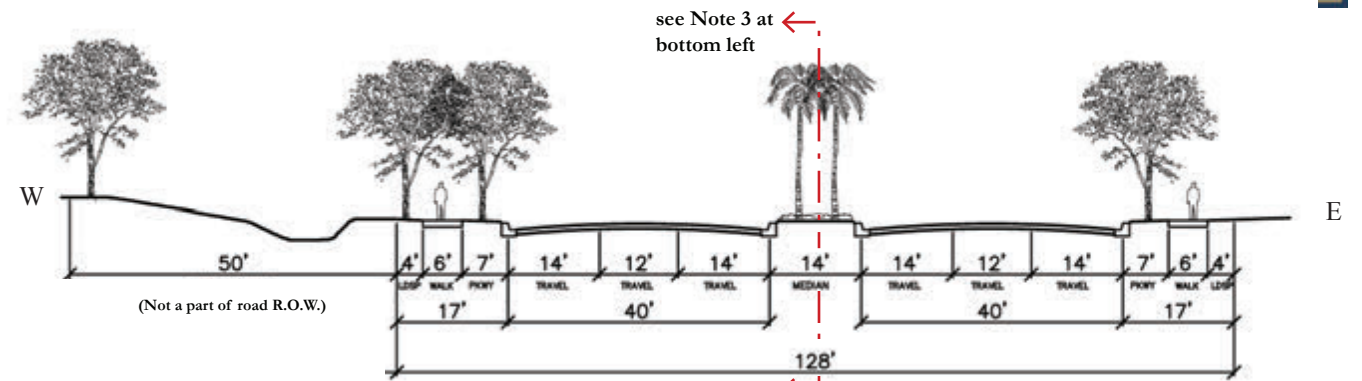
Vehicular Circulation Key

- █ Primary Arterial
- █ Secondary Arterial
- █ Collector
- █ Project Local
- █ Local

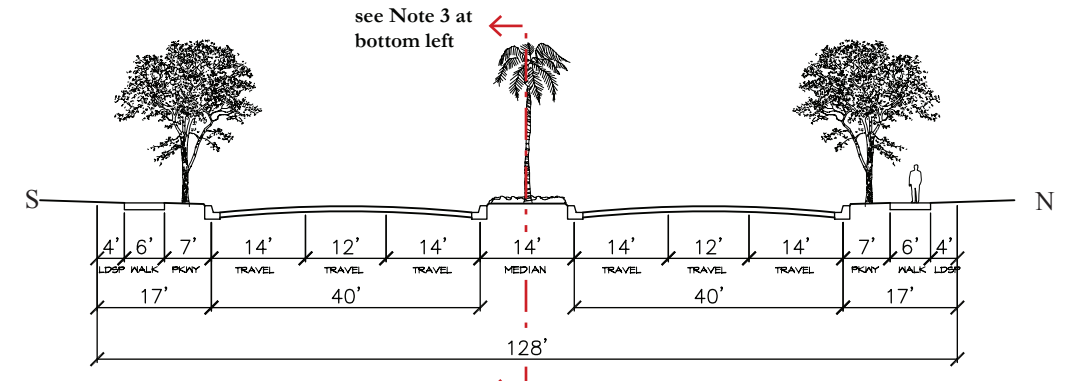
- Notes:**
1. Street sections continue on the next page.
 2. Parkway and landscape areas of six feet or less require root guards and/or plant species that will not harm the sidewalk.
 3. In instances where the street center line forms the perimeter boundary of Harvest Landing, right-of-way improvements will only be made on the project side of the street center line.

**Section A - Primary Arterial
PERRIS BLVD.**

Note: Actual Striping Plan may vary to reflect surrounding developments. Striping Plan will be approved by the city Traffic Engineer.



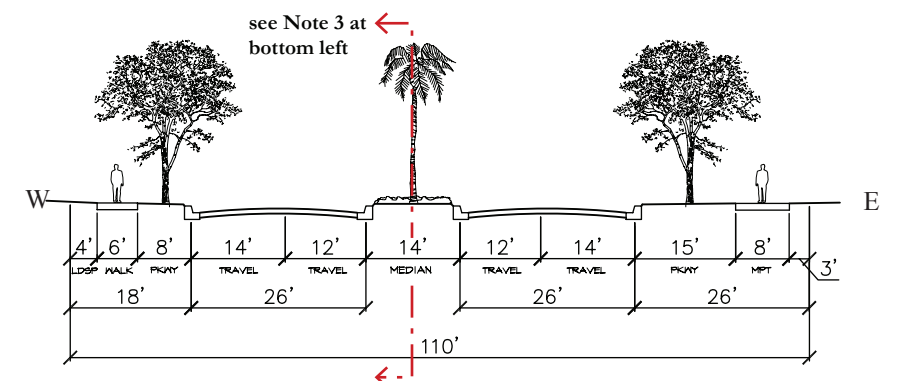
**Section B - Primary Arterial
PLACENTIA AVE.**



**Section C - Secondary Arterial
INDIAN AVE.**

(Placentia Ave. to Orange Ave.)

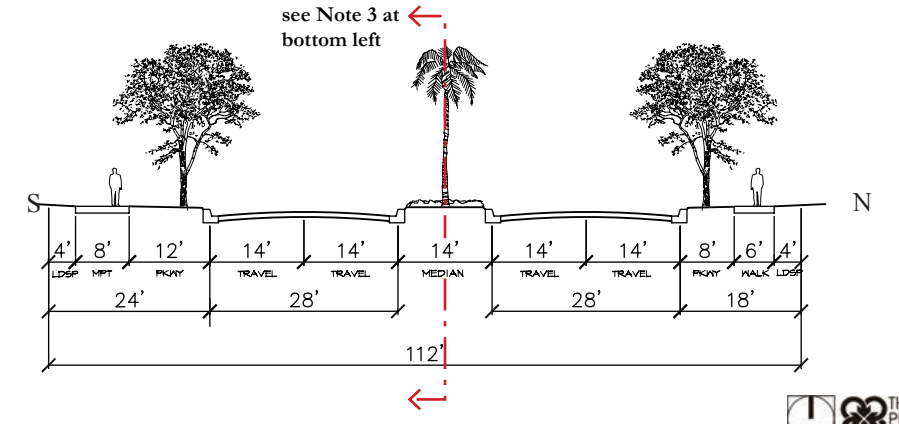
Note: The multipurpose trail should meander within the landscaped parkway area leaving a minimum of an 8' parkway on the roadway side and 3' on the outside edge.



**Section D - Secondary Arterial
ORANGE AVE.**

(Indian Ave. to Peris Blvd.)

- Notes:**
1. Adjacent to the sports park, the 4' outermost landscape area is not required.
 2. The multipurpose trail should meander within the landscaped parkway area leaving a minimum of an 8' parkway on the roadway side and 4' on the outside edge.

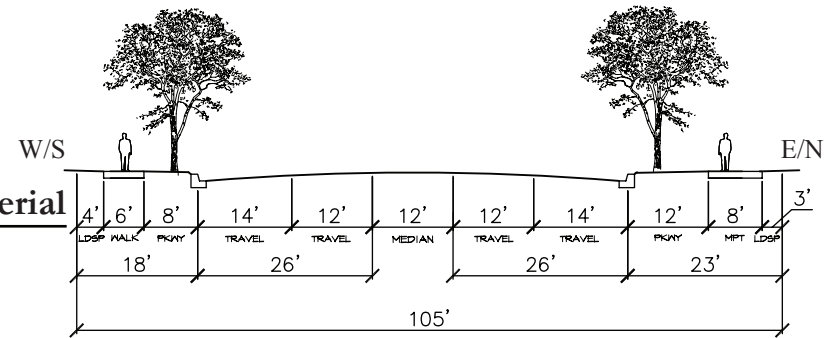


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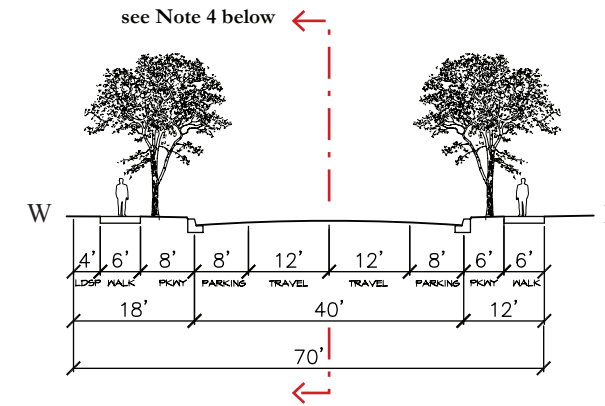
Section E - Secondary Arterial
INDIAN AVE.

(Orange Ave. to Perris Blvd.)

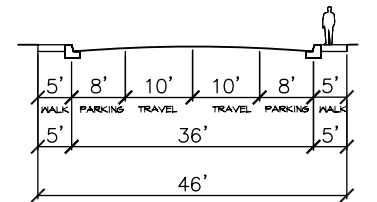


Note: The multipurpose trail should meander within the landscaped parkway area leaving a minimum of an 8' parkway on the roadway side and 3' on the outside edge.

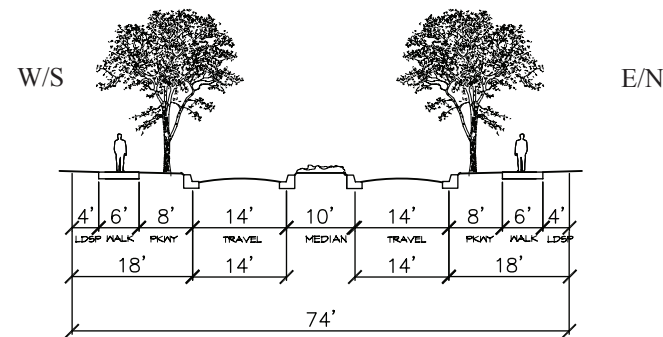
Section H - Local
BARRETT AVE.



Private Street A

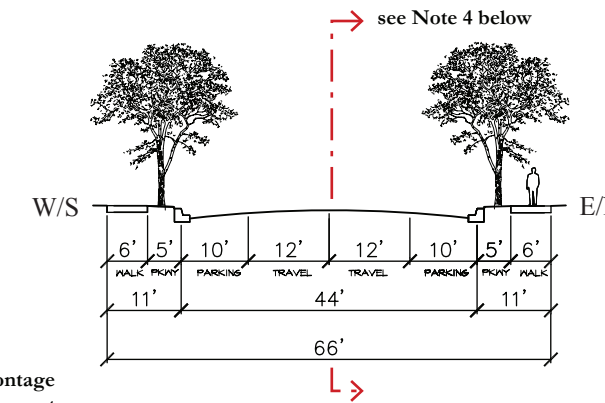


Section F - Local
PROJECT LOCAL (Entry)

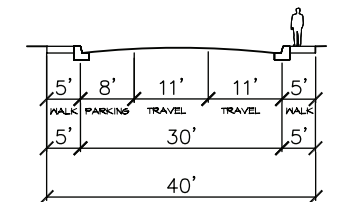


Section I - Collector
PROJECT
COLLECTOR

Notes: Between Indian Ave. and E. Frontage Rd., the 11' wide landscaped area on the western side of Barrett Ave. is not necessary due to the multipurpose trail in the adjacent paseo.



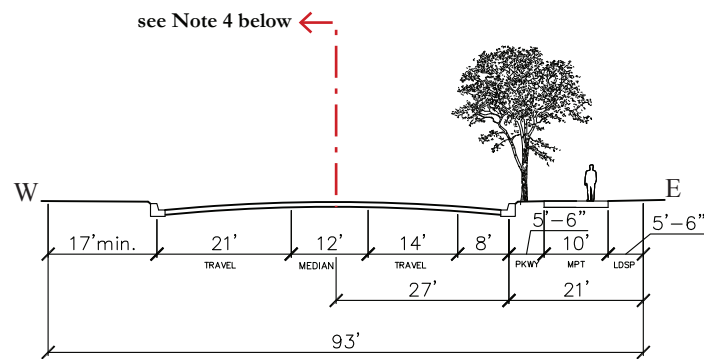
Private Street B



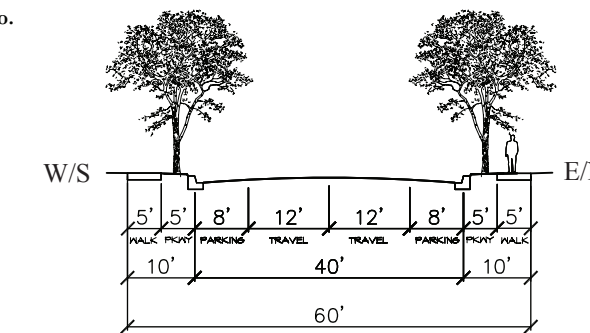
Section G - Collector
E. FRONTAGE RD.

(Placentia Ave. to Barrett Ave.)

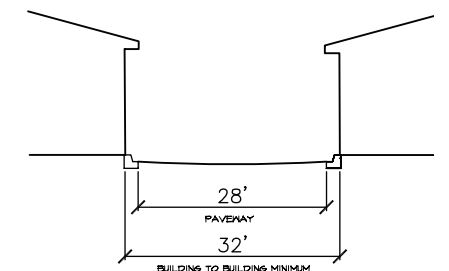
Note: For East Frontage Road, this section only applies if the MCP and I-215 widening are abandoned.



Local Roadway
Typical

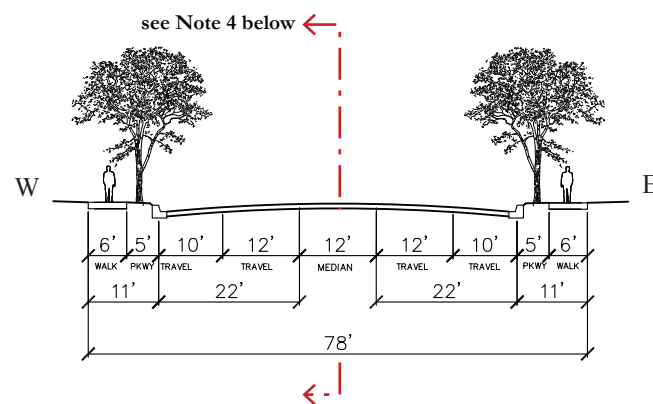


Alley



Section J - Collector
E. FRONTAGE RD.

(Barrett Ave. to Nuevo Rd.)

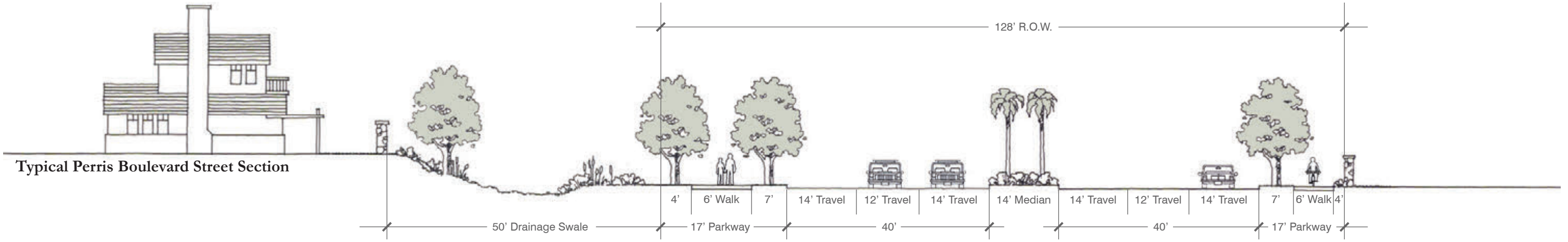


Notes:

1. Local roadways, private streets A & B, and alleys not shown on Vehicular Circulation Plan. These roadways will be developed with individual projects.
2. A minimum of a four-foot-wide planting area is required between the back of the sidewalk and a perimeter wall. In cases where the landscaped planting area adjacent to the back of the sidewalk indicated on the above cross-sections is larger than four feet, the larger number shall be utilized.
3. Parkway and landscape areas of six feet or less require root guards and/or plant species that will not harm the sidewalk.
4. In instances where a street center line forms a perimeter boundary of Harvest Landing, right-of-way improvements will only be made on the project side of the street center line.

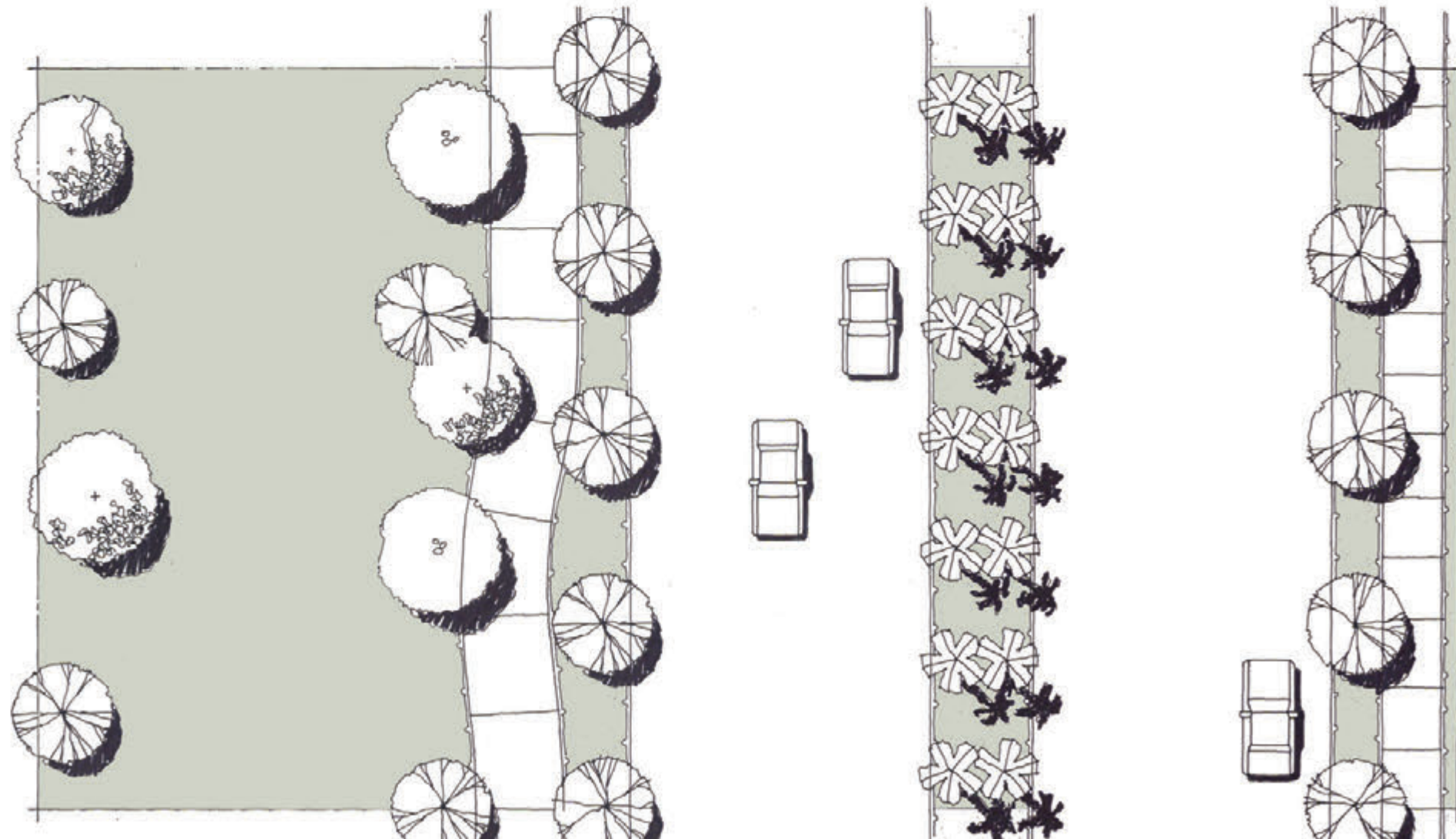


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Typical Perris Boulevard Site Plan

Note: Actual Striping Plan may vary to reflect surrounding developments. Striping Plan will be approved by the city Traffic Engineer.



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Secondary Arterials (Sections C–E)

Secondary arterials serve as the primary roadways within the Specific Plan area, carrying the majority of traffic into and throughout the project. Indian and Orange Avenues are the two secondary arterials in Harvest Landing and are further described below.

Indian Avenue (Placentia Avenue to Perris Blvd.)

Indian Avenue will serve as the main north-south corridor through the project. It will follow its current alignment and then curve eastward to intersect Perris Boulevard.

Indian Avenue serves both residential uses and business uses, which are located south of Orange Avenue along the western edge of Indian Avenue. Consequently, the eastern edge of Indian Avenue between Placentia Avenue and Orange Avenue has a 26-foot-wide parkway to create a pleasant pedestrian environment and to buffer residential uses from vehicular noise. The western edge has a consistent 18-foot-wide parkway to enhance the aesthetic appearance of the corridor and create a pedestrian-friendly buffer between the road and the adjacent multiple business use area. A meandering 8-foot-wide paved multipurpose trail in the eastern parkway and a 6-foot-wide trail/walk in the western parkway encourage bicycle and pedestrian movement. Roundabouts are allowed on Indian Avenue with approval of the City Traffic Engineer. See Figure 3-9 for a conceptual section and site plan of Indian Avenue north of Orange Avenue.

Between Orange Avenue and Perris Boulevard, the median is reduced from 14 to 12 feet and the parkway on the eastern and northern edges is reduced from 26 feet to 23 feet with a meandering 8-foot-wide multipurpose trail.

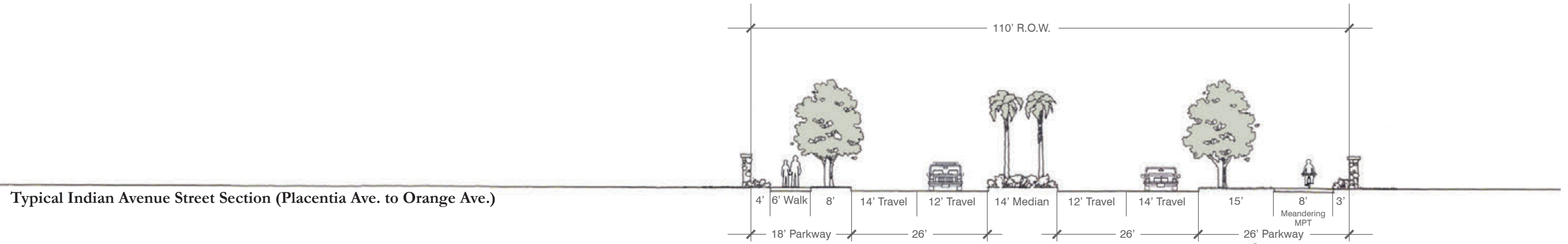
Orange Avenue

Orange Avenue will act as the primary east-west corridor in Harvest Landing and will be an important connector between the project and other areas in the City. Orange Avenue will be a 112-foot-wide right-of-way with four travel lanes and a median. The 24-foot-wide southern parkway contains an 8-foot-wide multipurpose trail to encourage pedestrian and bicycle circulation. See Figure 3-10 for a conceptual section and site plan of Orange Avenue.

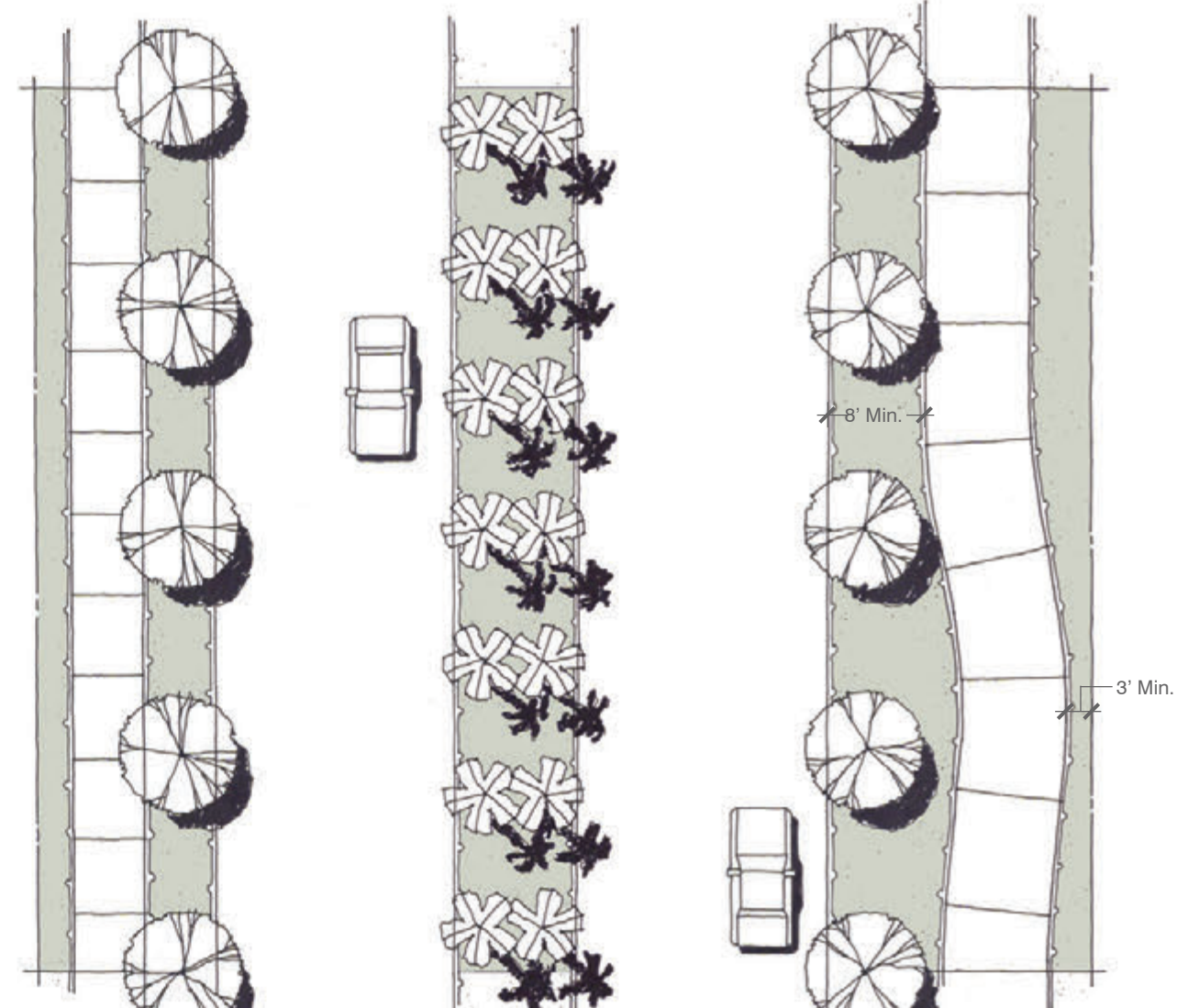
Indian and Orange Avenues

Typically, secondary arterials within the Harvest Landing Specific Plan have rights-of-way of 105-112 feet. They have four lanes of traffic with a 12-14-foot-wide landscaped central median and 18- to 26-foot-wide parkways. These wide parkways buffer residential uses and create a pedestrian-friendly environment along these important corridors.

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Typical Indian Avenue Site Plan (Placentia Ave. to Orange Ave.)



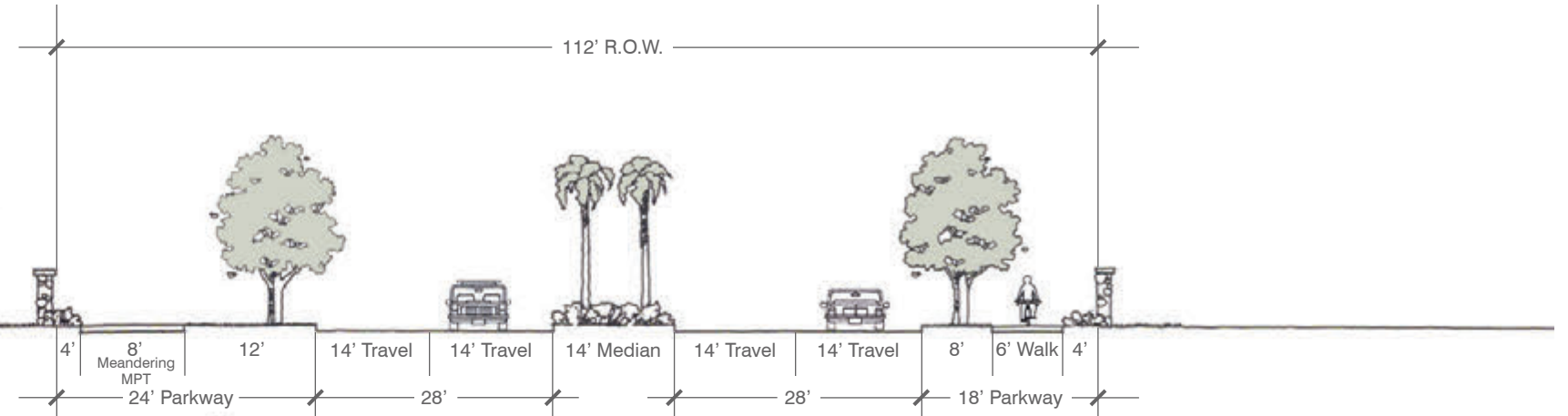
- Notes:
1. MPT = Multipurpose Trail
 2. The meandering MPT along the eastern edge of Indian Avenue should be located at least 8' from the back of the curb.



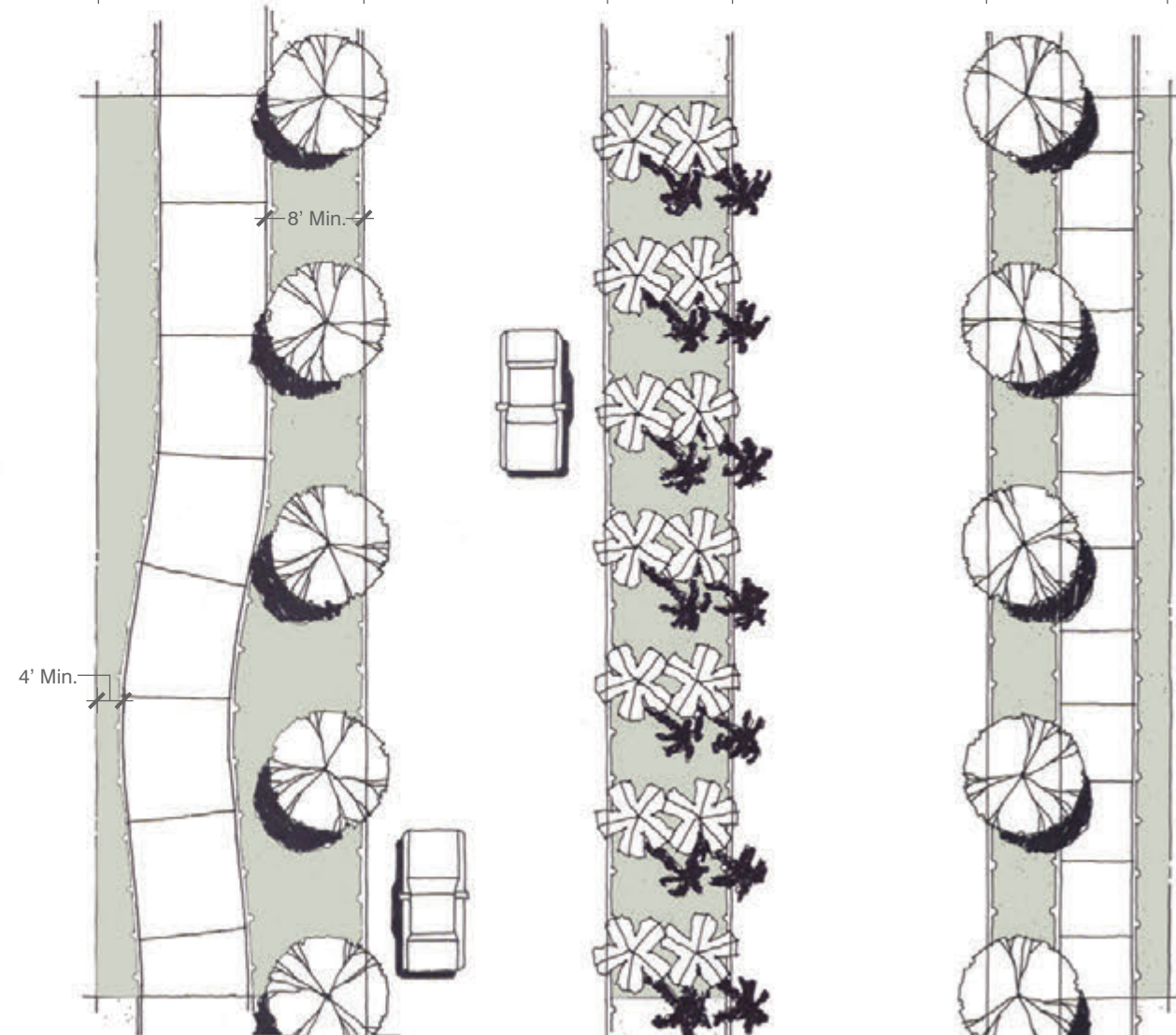
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Typical Orange Avenue Street Section



Typical Orange Avenue Site Plan



Notes:

1. MPT = Multipurpose Trail
2. The meandering MPT along the southern edge of Orange Avenue should be located at least 8' from the back of the curb.
3. The 4' planting area along the outside of the parkway is not required adjacent to the sports park.



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Collectors (Sections G and I)

Collectors serve as feeders to arterial and secondary arterial roadways within and surrounding the project.

With the exception of East Frontage Road, collectors are used primarily by local traffic to access residential and multiple business use areas. Collectors are designed to complement the character of surrounding uses. Accordingly, there are two types of collector roadways in the project site, including East Frontage Road and Project Collector. Each collector roadway is described in greater detail below.

East Frontage Road

East Frontage Road provides a connection between Nuevo Road and Placentia Avenue and provides access to I-215 by means of Nuevo Road. As proposed in Harvest Landing, East Frontage Road will be fully improved between Nuevo Road and Barrett Avenue and half-width improvements will be made between Barrett Avenue and Placentia Avenue. As noted below, a portion of East Frontage Road may be eliminated in conjunction with the MCP and I-215 widening. Accordingly, improvements to East Frontage Road will only be made if the proposed MCP and I-215 widening are either abandoned or reduced in scope so as not to eliminate this roadway.

Project Collector

The project collectors are designed to serve neighborhood traffic, connecting local and private streets with arterial level streets. These two-lane roads are intended to allow medium-speed traffic. They provide parking as well as an 11-foot-wide landscaped parkway with sidewalks on both sides of the street. Where residential collectors are adjacent to a perimeter wall, an additional 4-foot-wide landscape buffer is required between the edge of the sidewalk and the wall.

Local (Sections F and H)

The Harvest Landing Specific Plan contains both public and private local roadways. Local streets are typically located in or adjacent to residential neighborhoods and are primarily used by residents of those communities. With the exception of the project local roadway, all local streets in Harvest Landing are two lanes with parking on both sides and a right-of-way from 60 to 70 feet in width. Parkways of 10 to 18 feet in width encourage safe pedestrian movement within and between residential neighborhoods.

East Frontage Road

The planned future widening of I-215 in conjunction with the proposed Mid-County Parkway may result in the elimination of East Frontage Road north of Barrett Avenue. Harvest Landing has been designed to function with or without East Frontage Road. It is also important to note that planned future widening of Nuevo Road to its ultimate designation as a six-lane divided roadway with a raised median will result in limited access (e.g., right-in and right-out only turning movements) at the intersection of Nuevo Road and East Frontage Road.

Barrett Avenue

Barrett Avenue is a public, local roadway and is divided into two sections, bisected by Harvest Lake. The northern portion runs along the eastern edge of the project from Placentia Avenue to Orange Avenue. The southern portion runs south from Indian Avenue along the eastern edge of the project until it turns southwest and connects to East Frontage Road. An 18-foot-wide parkway with a 6-foot-wide sidewalk on the western edge of Barrett Avenue provides project residents with a safe and enjoyable pedestrian/bicycle linkage.

Project Local

Project locals serve as entries to the residential neighborhoods surrounding Harvest Lake. They are intended to foster a pedestrian-friendly environment with slower vehicular speeds than arterials. The project locals have two 14-foot-wide travel lanes with a 10-foot-wide median. Eighteen-foot-wide parkways with 6-foot-wide walks on both sides of the roadway create a safe and visually pleasing environment for pedestrians.

Local Roadways (Typical)

The street sections for local streets in Harvest Landing are identical to the City's General Plan future local standards. Local streets are found within the project's residential and multiple business use neighborhoods. They have two lanes with parking and a 10-foot-wide parkway on each side. The parkways include 5-foot-wide walkways to encourage pedestrian circulation. Local roadways are not depicted on the Vehicular Circulation Plan, as they will be the function of individual projects and approved during the tract map process.

Private Streets A and B

Private streets provide local access to a specific project rather than regional or community-wide movement. They have narrow travel lanes to encourage slow speeds. Rights-of-way are 40 feet wide for streets with parking on one side and 46 feet wide for streets with parking on two sides. Five-foot-wide walkways on both sides of the private streets provide for pedestrian movement. Private streets are not depicted on the Vehicular Circulation Plan, as they will be the function of individual projects and approved during the tract map process.



Alley

Alleys may be found in some residential neighborhoods depending upon the type of product. Alleys help to create a more attractive street scene in the front by moving the car to the rear of the building, which breaks the monotony of front-entry garage products with driveways along the street. The minimum pavement width for an alley is 28 feet. The minimum building-to-building separation along alleys is 32 feet. Alleys are not depicted on the Vehicular Circulation Plan, as they will be the function of individual projects and approved during the tract map process.

Roundabouts

Roundabouts may be strategically located along local roadways to enhance mobility and pedestrian safety. Roundabouts slow vehicular traffic more effectively than controlled intersections, helping to create more pedestrian-friendly roadways. In addition, the roundabouts are intended to discourage truck traffic through residential neighborhoods. Roundabouts have a minimum right-of-way diameter width of 96 feet with a 30 foot center median.

Mid-County Parkway

Background

At the time of adoption of this Specific Plan, the Riverside County Transportation Commission (RCTC) was studying the feasibility of a Mid-County Parkway (MCP) through the Perris area. The proposed 32-mile transportation corridor is intended to relieve vehicular congestion from the San Jacinto and Corona areas by providing an alternative to the SR-60 and SR-91 freeways.

On September 12th, 2007, RCTC identified the Placentia alignment as the locally preferred alternative. This is not a final decision nor is approval process completed. Additional steps include releasing the draft EIR/EIS, anticipated in 2008, and finalizing engineering designs, anticipated in 2009.

While at the time of the adoption of this Specific Plan an exact design for the MCP is not known, the RCTC provided a “worst-case” scenario, including a worst-case scenario for the widening of I-215 (controlled by Caltrans), that described the largest ultimate right-of-way necessary should the Placentia alignment alternative ultimately be selected. The right-of-way for the worst-case MCP is depicted on Figure 3-11 and involves: a new MCP located south of the existing



Examples of the types of traffic circles that may be utilized in Harvest Landing to control speeds and discourage truck trips.

Placentia Avenue; several ramps, overpasses, and bridges connecting with I-215; and a widening of I-215 to accommodate connecting ramps to the MCP. It should be noted that improvements to I-215 are under the jurisdiction of Caltrans and considered a separate project from the MCP.

Harvest Landing Circulation System with MCP

Accordingly, Harvest Landing has been designed to accommodate the worst-case MCP and I-215 widening scenarios, development of which requires 25.6 acres of land within the Specific Plan boundary including 15.7 acres of residentially designated land (High Density Residential) and 9.9 acres of Mixed Business Use land.

The Placentia Avenue MCP alignment could result in the widening of I-215 approximately 180 feet to the east, eliminating East Frontage Road through most of the project site. Harvest Landing's circulation system is designed to function in this potential event and connections to and reliance upon the East Frontage Road are avoided. Instead, Indian Avenue is connected to Perris Boulevard and Barrett Avenue provides a connection between East Frontage Road and Indian Avenue. These innovations allow north-south and east-west connections through and around Harvest Landing.

Until a determination is made and an official alignment selected, entitlement and development of Harvest Landing will continue under the assumption that the MCP will not impact Harvest Landing. The Phasing Plan, detailed in Section 5, *Implementation*, of this Specific Plan describes the intended construction schedule of Harvest Landing.

CIRCULATION SYSTEM WITH MCP



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Project Truck Routes

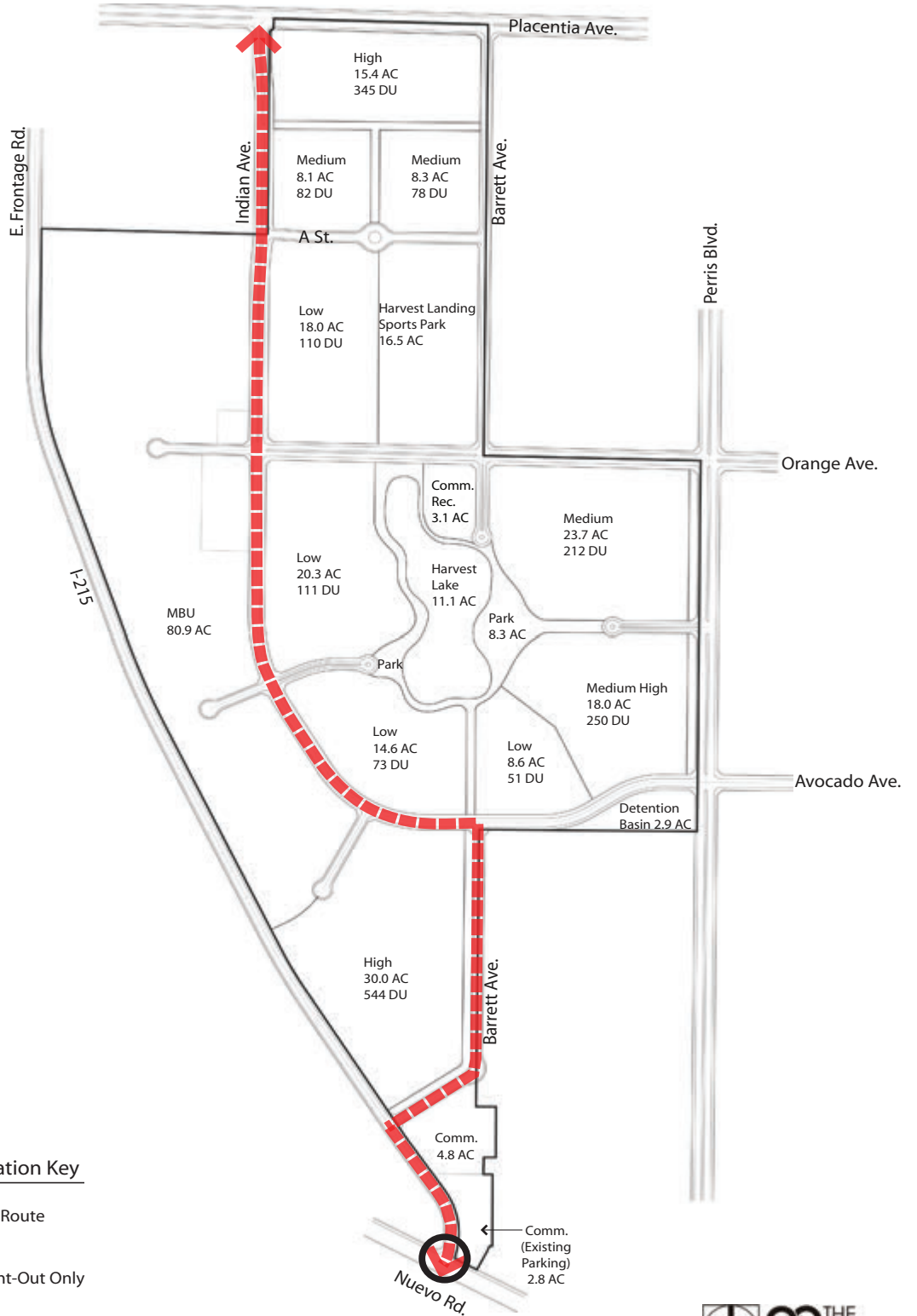
The truck routes in Harvest Landing are shown on Figure 3-12, *Project Truck Routes*. The Harvest Landing truck routes are consistent with Figure CE-15 of the General Plan, Future Designated Truck Routes, except that trucks have been re-routed to accommodate the potential worst-case scenario MCP Placentia alignment.

The truck traffic in Harvest Landing is routed along Indian Avenue, to Barrett Avenue, and then to East Frontage Road immediately north of the commercially designated portion. In this manner, consistency with the City's General Plan is maintained through Harvest Landing. The existing General Plan designated truck routes along Placentia Avenue, Nuevo Road, and Perris Boulevard remain unchanged and in effect as a result of this project.

In the event that the MCP worst-case scenario is developed and East Frontage Road is eliminated through a portion of the project north of Barrett Avenue, the project truck route will not be affected.

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PROJECT TRUCK ROUTES



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Pedestrian and Bicycle Circulation

An objective for the Harvest Landing Specific Plan is to create a lifestyle that promotes walking and bicycling and enhances the community's health and wellness. Accordingly, Harvest Landing includes an extensive system of trails, as shown in Figure 3-13, *Bicycle and Pedestrian Circulation Plan*.

The pedestrian and bicycle circulation system joins important community features, such as the lake, the recreation center, and the Harvest Landing Sports Park, creating an off-street pedestrian and bicycle linkage connecting the northernmost residential area with the southern commercial area. This pedestrian and bicycle linkage promotes safe and enjoyable nonvehicular movement throughout the entire project and between the project and adjacent uses.

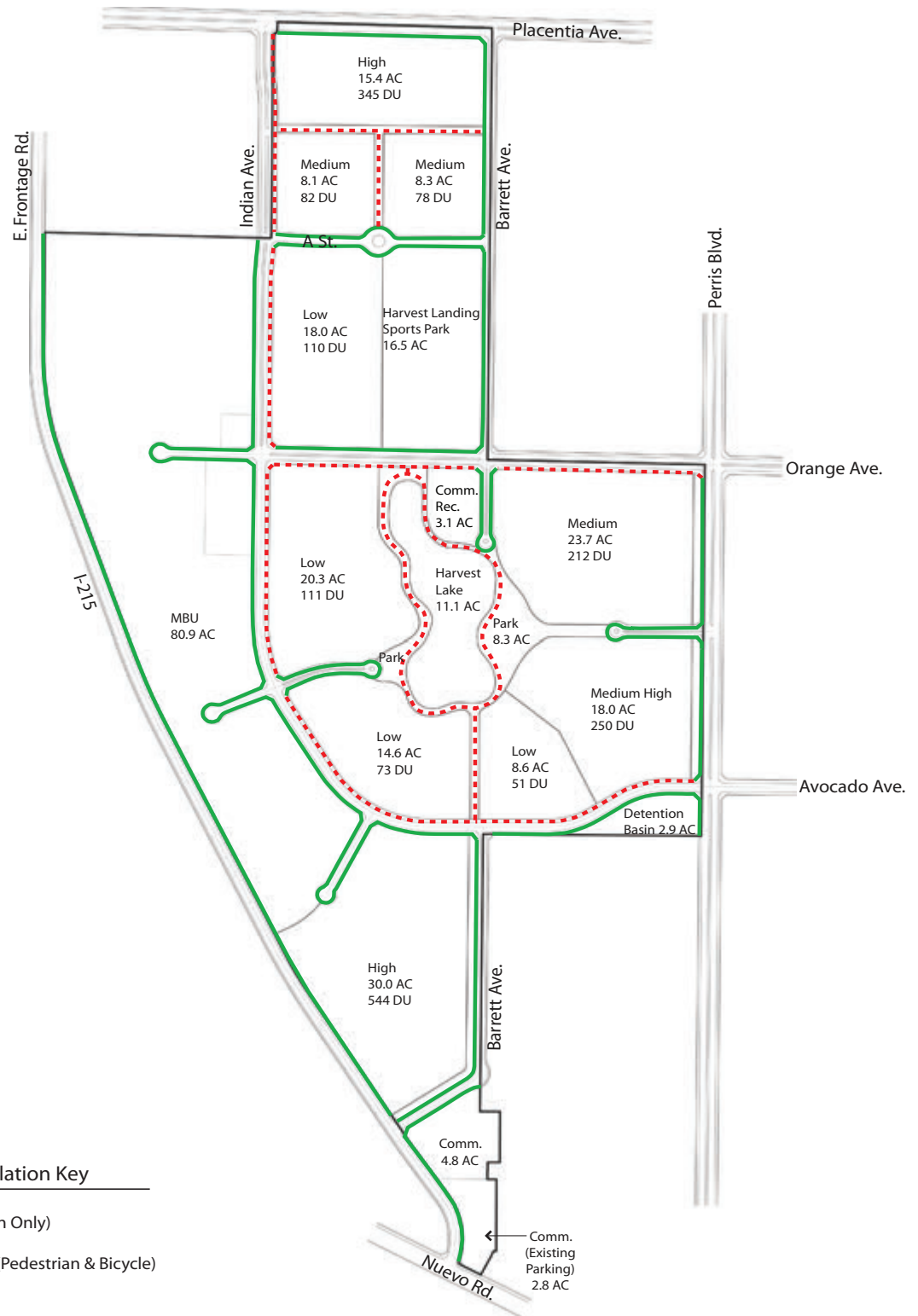
As shown on Figure 3-13, the pedestrian circulation system is composed of sidewalks and multipurpose trails. Sidewalks are adjacent to the roadway and multipurpose trails are located within paseos, which can either be street-adjacent or off-street. The paseos are described in the Parks and Open Spaces section later in this document.

The City's climate makes bicycling a viable year-round recreational opportunity and alternative mode of transportation. Bicycle circulation in Harvest Landing occurs on the roadways and multipurpose trails as follows:

- Class I (off-road)—are shared pedestrian and bicycle trails classified as multipurpose trails in this Specific Plan. The multipurpose trails are located in the street-adjacent paseos on the east side of Indian Avenue between Placentia Avenue and Perris Boulevard and the south side of Orange Avenue between Indian Avenue and Perris Boulevard. Multipurpose trails, including their class I bikeways, are also located in all of the interior paseos.
- Class III bikeways (non-striped on-street)—are shared bicycle and vehicular roads identified only by signage. Class III routes are located along all secondary arterials, collectors, and local roadways.

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BICYCLE AND PEDESTRIAN CIRCULATION PLAN



- Pedestrian & Bicycle Circulation Key**
- Sidewalk (Pedestrian Only)
 - - - - Multi-Purpose Trail (Pedestrian & Bicycle)



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Transit

Currently, two Riverside Transportation Agency (RTA) bus lines provide service to the project area. Route 19 provides service from the Moreno Valley Mall to downtown Perris, with a transit point at the Valley Plaza Hospital (on Perris Boulevard near Orange Avenue). Route 30 runs from the Perris Valley Spectrum (located due north of the eastern portion of the project site) to the intersection of Weston and Carter. This route provides service to the project area through stops at the Perris Valley Spectrum and Valley Plaza Hospital (both located along Perris Boulevard north of Orange Avenue) and at Perris Boulevard and Nuevo Road. A major transit center is planned for downtown Perris. This transit center will provide both RTA bus and MetroLink service, dramatically improving transit access throughout the region from the project site.

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Parks and Open Spaces

Introduction

This section details the park and open space plan for Harvest Landing. A discussion of the City's park requirements and needs precedes a description of the recreational amenities provided for in Harvest Landing.

City of Perris Park Plan and Requirements

The City of Perris has detailed park requirements that set the minimum parameters for the recreational amenities provided in Harvest Landing. Specifically, the City of Perris General Plan and Parks and Recreation Master Plan sets a park standard of five acres of parkland per 1,000 residents. Harvest Landing will help the City toward achievement of its overall parkland goal because it exceeds the Plan's requirements.

The Parks and Recreation Master Plan, which was approved August 30, 2005, describes the recreational needs, goals, and standards of the community. According to the Parks and Recreation Master Plan:

- There is a desire to design parks that “have the potential to generate revenue from facility rentals and concessionaires” to offset a portion of costs to maintain and operate City facilities.
- It is a priority to develop active sports facilities.
- The City needs to develop a trail system to link parks, schools, and recreation facilities.
- Parks need to be designed to be safe and secure.
- Parks must be well maintained.

The Parks and Recreation Master Plan defines recreational facilities (see sidebar) and also defines what the City does and does not consider to be parkland. Specifically, according to Action item 10.1, the City does not consider the following features as parkland:

- Planning area edges
- Landscaped community or subdivision entries or medians
- Meandering streams, fountains, or other water features
- Paseos, greenbelts, trails, walkways, setbacks, and other similar features that are used for transportation and are not destinations in and of themselves

Parks and Recreation Master Plan

The City of Perris Parks and Recreation Master Plan defines recreational facilities as the following:

- **Community parks are between 15 and 40 acres in size and provide a variety of activities such as athletic fields, community/recreation centers, and picnic areas. The amenities of a community park are expected to serve the residents within a 1½-mile radius of the park.**
- **Neighborhood parks are between 5 and 14 acres in size and are intended to provide for the daily recreation needs of residents in the immediate vicinity of the park. Primary activities can include passive and open space, active play areas, picnic areas, and practice playfields with lighted facilities. The amenities of a neighborhood park are expected to serve the residents within a ½-mile radius of the park.**
- **Mini parks serve to accommodate immediate neighborhood areas by providing such amenities as play areas, play equipment, barbecues, and picnic benches. The City does not provide standards for the development of mini parks.**

- Streetscapes
- Easements
- Sites with drainage structures (box channels, swales, etc.) designed for less than a 10-year storm event
- Private park facilities less than four acres in size, unless determined to fulfill a need or facility requirement for a specific neighborhood

Harvest Landing Park Plan

As shown on Figure 3-14, *Parks and Open Space Plan*, Harvest Landing includes a wealth of open spaces that serve multiple functions: as recreational opportunities, as community separators, as buffers, as a community theme, and as an interconnecting system of trails. These open spaces are a valuable asset to the community and, significantly, to the City of Perris as well. As noted, Harvest Landing will not only help the City to achieve its overall park acreage goals, but fulfill the goals of the Parks and Recreation Master Plan.

The parks are easily accessible to every resident and business within the Specific Plan. Parks are located to ensure that all homes are within one-quarter mile and all other areas of the specific plan are within one-half mile of a park. The parks are interconnected by the system of paseos, which are also described earlier in this Section.

PARKS AND OPEN SPACE PLAN



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Maximum build-out of the Harvest Landing Specific Plan would accommodate 1,860 future units and a population of approximately 6,938 residents. Based on the City’s standard of five acres of parkland per 1,000 residents, full build-out of the Specific Plan would result in the need to provide 34.7 acres of parkland or an equivalent fee in lieu of dedicated parkland. Harvest Landing exceeds the City’s requirement of parkland by over one acre. A total of 35.9 acres of public parkland and 8.1 acres of private recreation and paseos are provided in Harvest Landing, as summarized in Table 3-8 and further described below.

Harvest Landing intends to develop the public parks and recreational facilities listed in Table 3-8 and dedicate the Harvest Landing Sports Park to the City in a fully improved state. This achieves Action Item 1.3 of the Parks and Recreation Master Plan, which states, “While requiring the ratio of five acres of parkland per 1,000 population in the City, recognize that the City Council may want to trade off some required acreage in exchange for dedication of *developed* park sites, where appropriate, instead of receiving just undeveloped parkland.”

Table 3-8
Parks and Recreation Facilities Summary ¹

Public Park/Recreation Facility	Acres
Harvest Lake Park	8.3
Harvest Lake	11.1
Harvest Landing Sports Park	16.5
<i>Subtotal Public Parks</i>	<i>35.9</i>
Private Parks and Paseos	Acres
Community Recreation Center (private)	3.1
Internal Paseos	5.0
<i>Subtotal Private Parks and Paseos</i>	<i>8.1</i>
Total	44.0

Notes:

¹ The actual park area credited toward satisfaction of the City’s park requirement will be determined through separate negotiations and memorialized in the Development Agreement approved by the Developer and City.

Harvest Landing consists of the following recreational facilities.



One type of active use envisioned in Harvest Landing Sports Park.



Harvest Lake and Harvest Lake Park will form the iconic image of the development.

Harvest Landing Sports Park—This 16.5-acre active, community park is adjacent to Orange Avenue and, in conjunction with Harvest Lake, forms a recreational core for the community. Helping to satisfy the City’s desire for sports parks, the Harvest Landing Sports Park accommodates facilities such as illuminated baseball and soccer fields, basketball courts, recreation centers, play grounds and tot lots, and picnic areas. See Figure 3-15, *Harvest Landing Sports Park Conceptual Site Plan*, for a conceptual plan for the types of facilities accommodated in this park; however, final design will be coordinated with City staff.

Portions of the Harvest Landing Sports Park will also be used for the detention of flood waters and will be inundated during 100-year storm events. This fulfills Action Item 1.6 of the Parks and Recreation Master Plan, which states, “Explore ways to utilize catch basins and drainage areas for open space and park uses, even if they flood periodically during the rainy season.” After improvement and dedication, this park will be owned and maintained by the City.

Facilities for active recreational uses that are likely to draw cheering crowds, elicit loud play, have amplified game announcements (i.e., stadiums, soccer fields, tennis courts, basketball courts, etc), or are illuminated for night-time play should be located within the park’s interior and away from surrounding residential uses. Lights and speakers shall be directed away from residences to minimize light and glare impacts. Lights shall be hooded, positioned, and directed toward the field of play to minimize light spillover. Any speakers should be located near the ground and distributed evenly to minimize the need for high volume. Speakers and lights should be turned off by 10:00 PM.

Harvest Lake—The 11.1-acre Harvest Lake is the central feature of Harvest Landing. Not only is it the signature visual amenity of the community, it forms the main theme of the Plan. Harvest Lake is a recreational feature as much as any land-based park. It accommodates nonmotorized uses such as sailing, fishing, canoeing, and paddle boats and is intended to be used by the entire Perris community. However, swimming is not allowed. Water quality in Harvest Lake will be maintained by facilities such as:

- Biofilters, which strip water of nutrients that promote algae growth.



- Aeration systems, which introduce oxygen into the lake to increase dissolved oxygen levels and promote the natural convection of water.

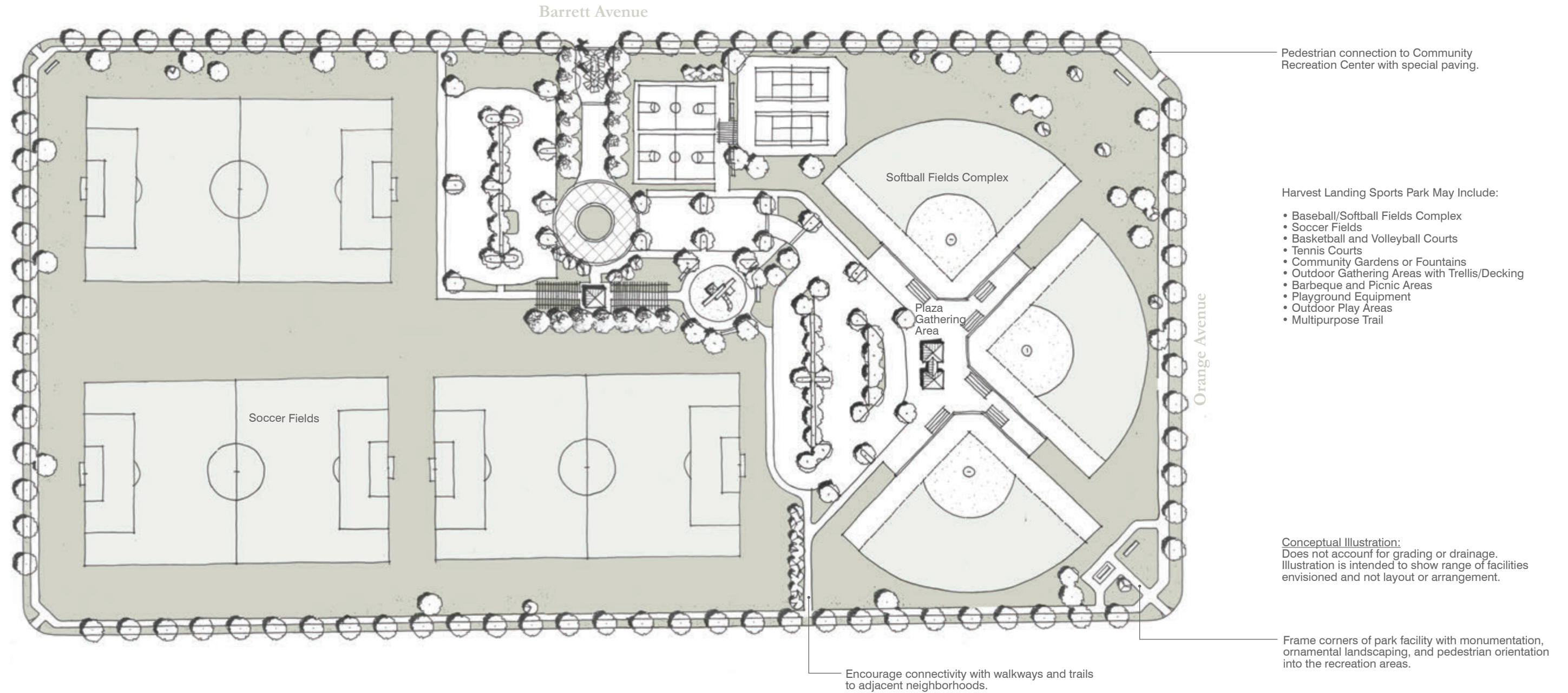
Harvest Lake Park—Harvest Lake Park surrounds Harvest Lake and undulates approximately between 30 and 70 feet in width. As shown in Figures 3-16, *Harvest Lake Park Conceptual Site Plan*, and 3-17, *Harvest Lake Conceptual Section*, Harvest Lake will accommodate both active and passive recreation. It contains an 8-foot-wide multipurpose trail that meanders around the Lake so that residents and visitors can stroll, jog, or utilize exercise equipment. Sitting and picnic areas will be strategically located throughout the park.

Community Recreation Center—In conjunction with Harvest Lake, this 3.1-acre active recreation facility forms the hub of activity for the community. The private Community Recreation Center may include recreational amenities such as a clubhouse, tennis courts, and pool. Figure 3-18, *Community Recreation Center Conceptual Site Plan*, shows a conceptual site plan for the recreation center.



The private community recreation facility will act as an activity center for the residents of Harvest Landing.

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Note: Illustration is conceptual in nature and subject to change. Park features and field orientation to be determined at the time of detailed park design.



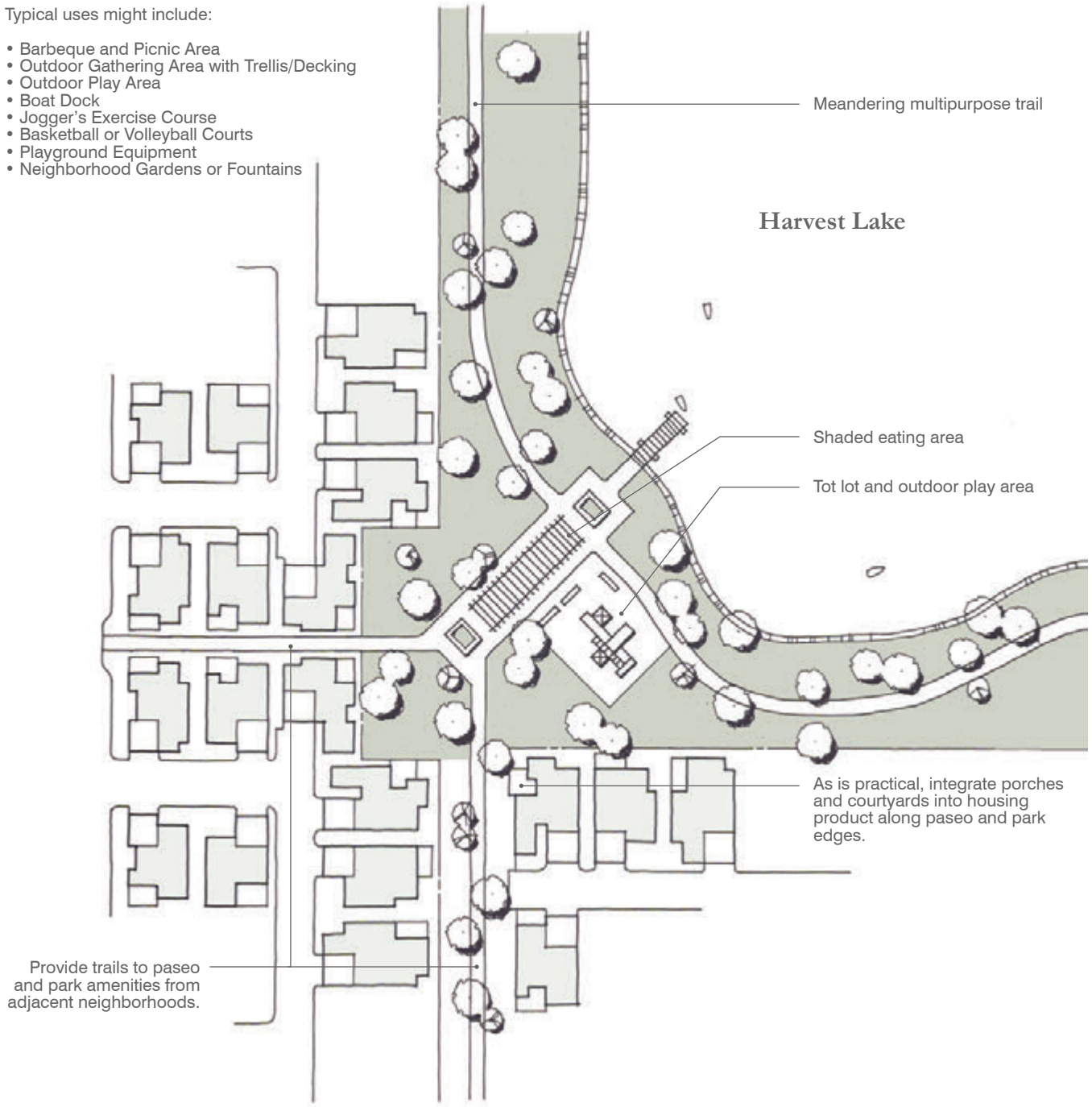
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HARVEST LAKE PARK CONCEPTUAL SITE PLAN



Typical uses might include:

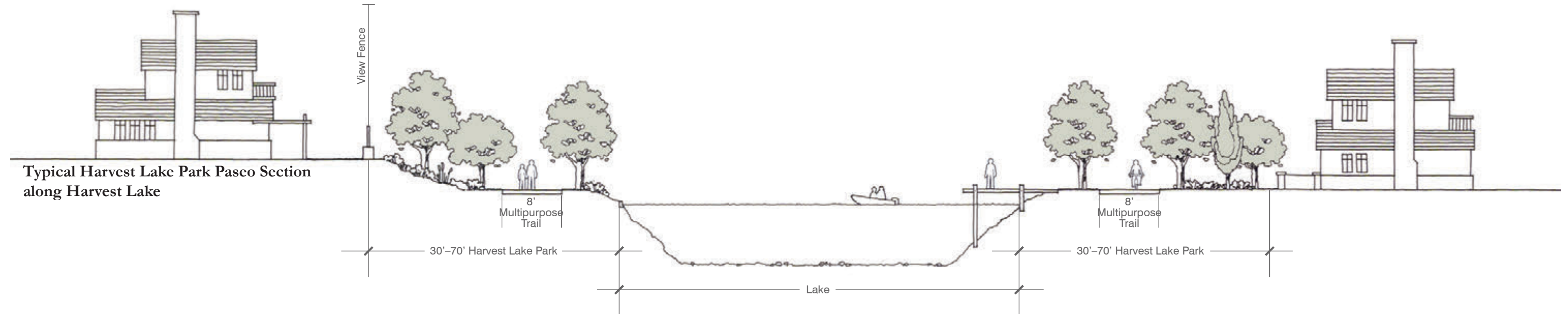
- Barbeque and Picnic Area
- Outdoor Gathering Area with Trellis/Decking
- Outdoor Play Area
- Boat Dock
- Jogger's Exercise Course
- Basketball or Volleyball Courts
- Playground Equipment
- Neighborhood Gardens or Fountains



Note: Illustration is conceptual in nature and subject to change. Park features to be determined at the time of detailed park design.



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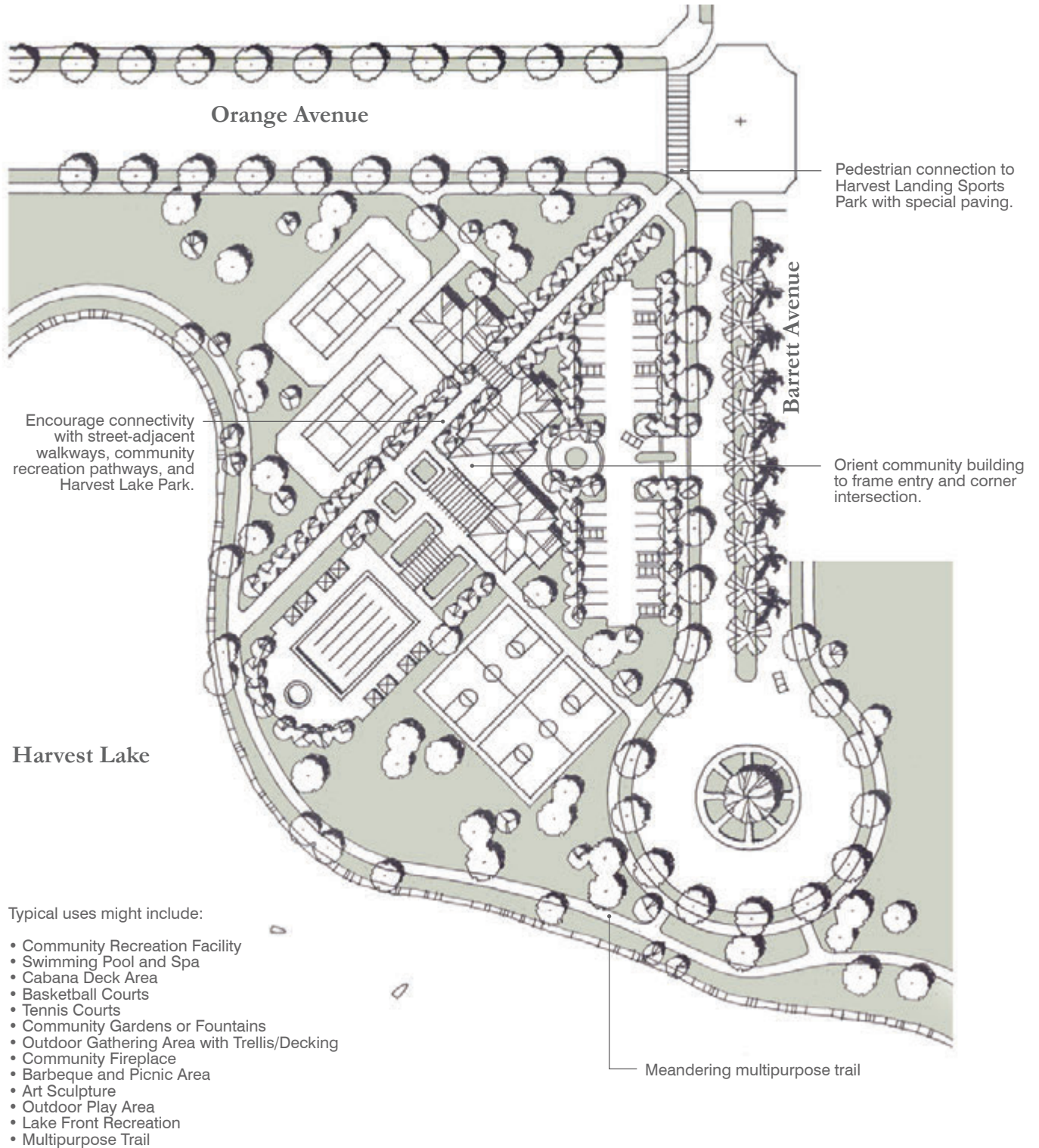


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COMMUNITY RECREATION CENTER CONCEPTUAL SITE PLAN



Note: Illustration is conceptual in nature and subject to change. Park features to be determined at the time of detailed park design.



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Paseos—Running throughout Harvest Landing is an interconnected system of paseos that contain meandering multipurpose (walking/cycling) trails, landscaping, entry and community theme features, and exercise equipment. These paseos satisfy the City’s stated desire to develop trail systems linking the community as they connect residential areas, parks/open spaces, the community recreation center, and the multiple business use area as well as provide a separation between neighborhoods.



Two types of paseos exist within Harvest Landing: roadway paseos and internal paseos. As shown in Figure 3-13, roadway paseos are located adjacent to Indian and Orange Avenues, varying from 18 to 26 feet in width (see street sections for sizes of each roadway paseo). An example of roadway paseos can be seen in Figure 3-19. Internal paseos consist of landscaped areas with 8-foot-wide multipurpose, meandering trails. Internal paseos are a minimum of 50 feet wide and are located between neighborhoods.



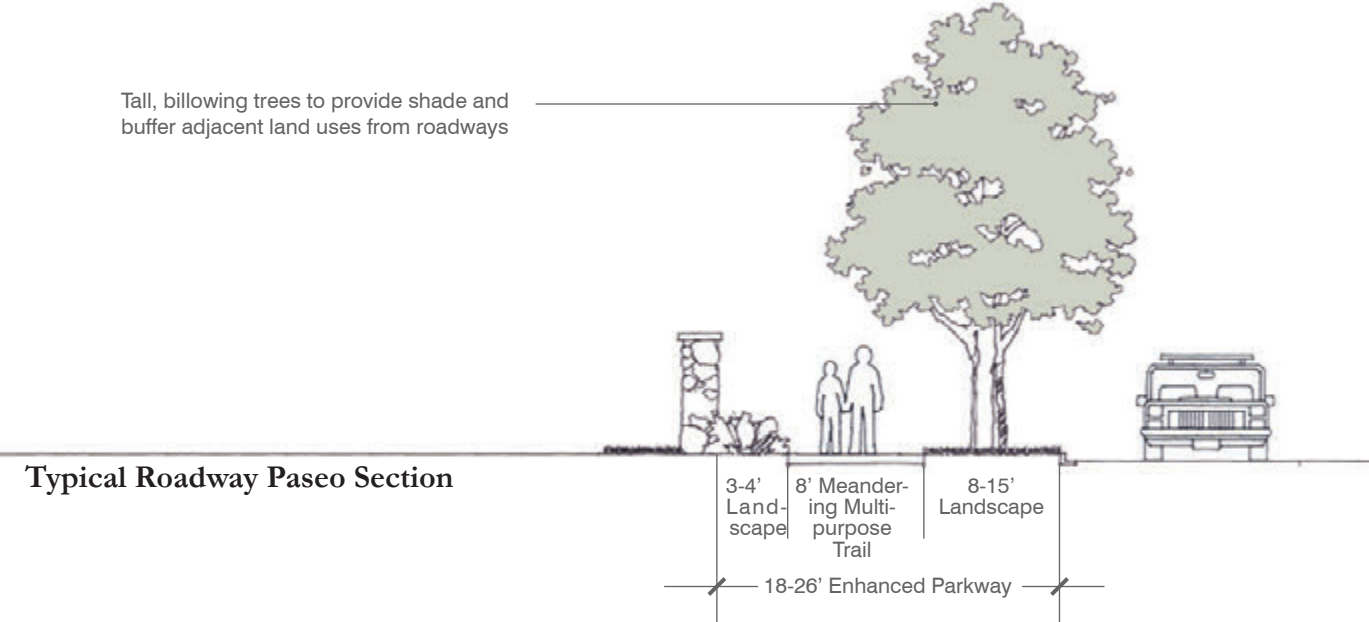
Paseos serve to separate neighborhoods, improve connectivity, and provide recreational opportunities.

Paseos will include a variety of amenities to add interest and encourage their use. Such amenities could include, but are not limited to:

- Rest stations, including benches, shade structures, and water fountains
- Exercise stations consisting of elements such as pull-up bars, stretching benches, steps, health information signage, etc.
- Picnic areas
- Tot lots
- Shade trellises
- Bollard lighting
- Directional signage
- Informational kiosks

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ROADWAY PASEOS



Note: Illustrations are conceptual in nature and subject to change. Paseo features to be determined at the time of detailed paseo design.



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Grading and Infrastructure Plans

Introduction

This section details the on- and off-site plans for the grading, storm drainage, water, and sewer systems necessary to accommodate Harvest Landing. Harvest Landing is serviced by an extensive and well-planned infrastructure system. The grading and water, wastewater, and drainage systems are designed to serve the development during construction and at build-out and connect to the regional/local systems.

Grading Plan

Harvest Landing is relatively flat, sloping eastward from I-215 at an approximately 1% gradient. Grading is still necessary to create Harvest Lake, to accommodate proper drainage and water detention, and to create level building pads.

Figure 3-20, *Preliminary Grading Plan*, illustrates the preliminary grading plan for Harvest Landing. This plan illustrates how the grading will be balanced on-site without reliance on the importing or exporting of extra material. The preliminary earthwork calculations estimate that approximately 1.3 million cubic feet of material will be used to grade the site.

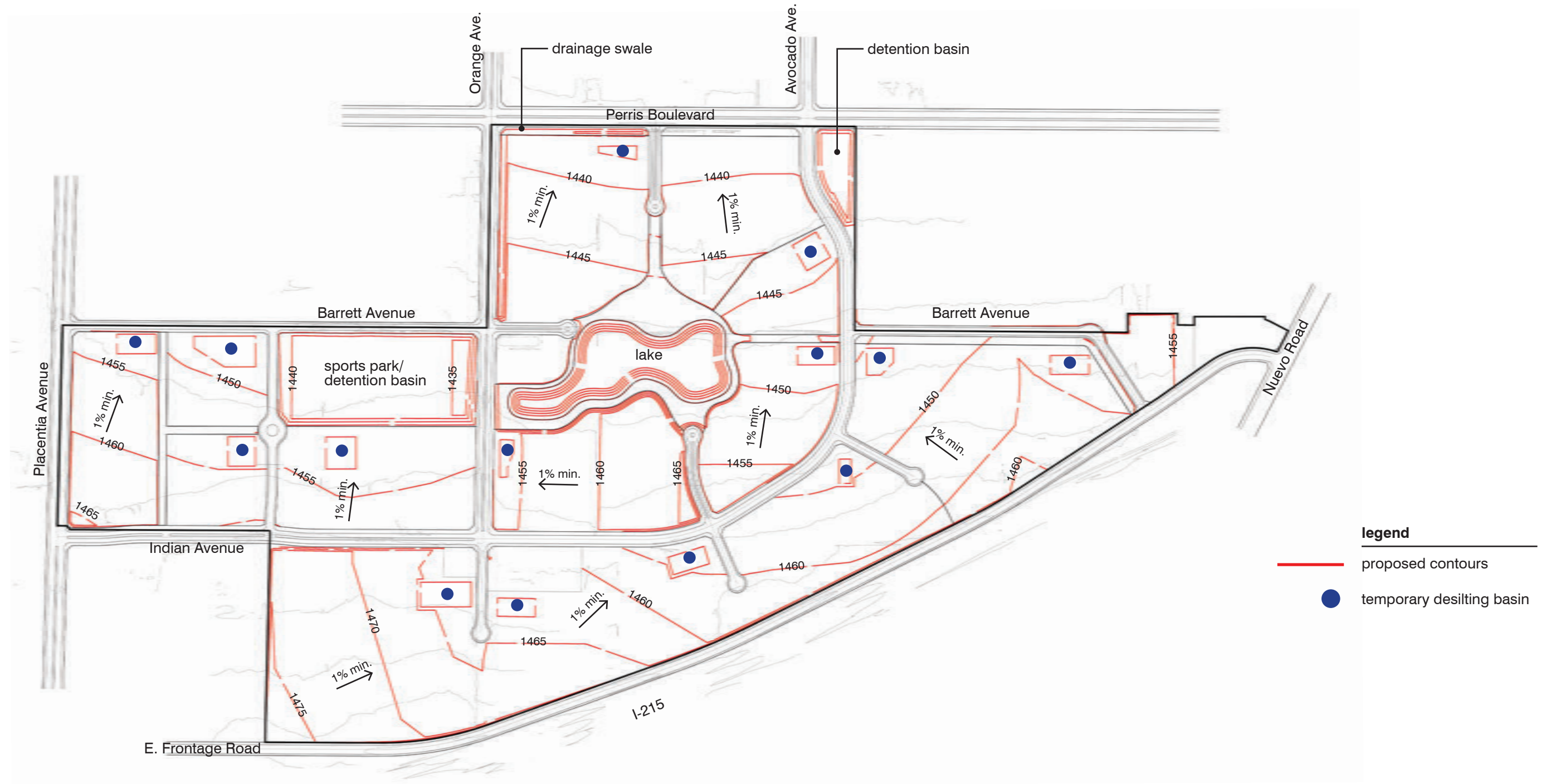
Significant grading features of Harvest Landing include Harvest Lake and the Harvest Landing Sports Park. Harvest Lake is proposed at a depth of 25 feet and Harvest Landing Sports Park is proposed at a depth ranging from 10 feet on its northern end to 16 feet at its southern end.¹ The lake and sports park generate a significant amount of cut to help balance the earthwork demand for the project. All development areas are designed with positive drainage that follows existing topographic flow towards off-site drainage conveyances.

It is important to note that Figure 3-20 is preliminary and refinements will occur as more detailed development plans are prepared. Grading plans shall be approved by the Riverside County Flood Control and Water Conservation District (RCFC&WCD) and reviewed by the Eastern Municipal Water District (EMWD) prior to the

¹ The actual park area credited toward satisfaction of the City's park requirement will be determined through separate negotiations and memorialized in the Development Agreement approved by the Developer and City.

issuance of a grading permit by the City of Perris. In addition, grading within Harvest Landing will be subject to the following standards:

- Prior to any development, the Conceptual Grading plan for each tentative tract map shall be submitted to the City for approval. The Grading Plan for tentative tract map shall be used as a guideline for subsequent detailed grading plans for individual developments and shall include preliminary pad and roadway elevations.
- All grading shall be in substantial conformance with the overall Conceptual Grading plan, the uniform Building Code, Chapter 70, and Riverside County Ordinance No. 457.
- Unless otherwise approved by the City of Perris, all cut and fill slopes shall be constructed at inclinations of no steeper than two (2) horizontal feet to one (1) vertical foot.
- Prior to grading, a grading permit shall be obtained from the City of Perris.
- Soil stabilizers shall be used to control dust as required by SCAQMD Rule 403.
- Erosion control practices shall be implemented during grading activities.
- The grading contractor shall be required to obtain an encroachment permit from EMWD, CALTRANS, or RCFC&WCD prior to entering the right-of-way of any of these agencies.
- All construction activities, including clearing, grading, or excavation, which results in the disturbance of at least one acre total land area, or activity that is part of a larger common plan of development of one acre or greater shall obtain the appropriate NPDES construction permit.



Note: Illustration is conceptual in nature and subject to change.



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Storm Drainage Plan

Existing Drainage

Prior to development of Harvest Landing and surrounding master planned drainage improvements, a total of 977 acres of drainage (341 on-site and 636 off-site from the west and north) and a peak flow rate of approximately 1,090 cubic feet per second (cfs) impacted the site. Of the total existing drainage area and flow, 27 acres and 40 cfs drain to Line J and the remaining 950 acres and 1,050 cfs drain to Line K. Prior to implementation of Harvest Landing and Perris Valley Master Drainage Plan (PVMDP) improvements, off-site drainage came from four sources:

- Culvert 1 (Orange Avenue): a 4' x 3' reinforced concrete box that accommodated 240 cfs of off-site drainage from approximately 130 acres to the west. Water from Culvert 1 flowed into an existing lateral and headwall at the intersection of Orange and Barrett Avenues via a natural earthen channel.
- Culvert 2 (approximately 1400' South of Orange Avenue): a 6' x 3' reinforced concrete box that accommodated 360 cfs of off-site drainage from approximately 380-acres to the west.
- Culvert 3: Line J, which runs through the southern portion of the site and conveys off-site drainage from the west through Harvest Landing via an underground pipe.
- Northwest: Approximately 70 acres and 64 cfs of surface flows from the northwest.

Proposed Drainage System

Development of Harvest Landing will require the collection and conveyance of on-and off-site storm flows in a manner that will ensure protection of the site and downstream properties. As a part of the development of Harvest Landing, storm flows will be collected and routed through a series of catch basins and storm drains that will convey water through two on-site detention basins and ultimately to Lines K and J of the PVMDP in a manner that does not exceed the maximum capacity for these facilities, as shown on Figure 3-21, *Storm Drainage Plan*.

Harvest Lake

It should be noted that Harvest Lake is not a part of the storm drain system. Harvest Lake is a self-contained system and is not connected to the drainage system. The lake will have an emergency overflow pipe system that conveys flows to line K in the event of high water levels.

On-Site Conveyance System

On-site drainage will be routed through three separate storm drain systems as follows:

- Systems 1 and 2 drain 314 acres of Harvest Landing through two detention basins (discussed below) and ultimately into Line K. System 1 drains to the Harvest Landing Sports Park detention basin and System 2 drains to the detention basin located on Perris Boulevard.
- System 3 drains 27 acres in the southern portion of Harvest Landing to Line J.

Off-Site Drainage System

Line K

Per the PVMDP, Line K can accommodate a maximum flow of 841 cfs. As proposed, Harvest Landing releases a maximum flow of 687 cfs into Line K, which, when combined with the 154 cfs of existing off-site flows in Line K, comply with the PVMDP requirement for Line K.

As a part of Harvest Landing, Line K will be improved. Between Perris Boulevard and Medical Center Drive, Line K will be improved to an 18' x 7' concrete rectangular channel. Under Medical Center Drive, Line K will be improved to an 18' x 6' reinforced concrete box.

Line J

Line J collects off-site drainage from the west and 60 cfs of on-site drainage from 27 acres in the southern portion of Harvest Landing. Line J is built to PVMDP capacity and will not be improved as part of Harvest Landing.

On-Site Detention Basins

To mitigate the flow from the site to Line K down to the required 687 cfs, the two on-site detention basins shall have a minimum capacity to store 92 acre-feet of storm water; however, as preliminarily designed, the Harvest Landing Sports Park and Perris Boulevard detention basins have a preliminary storage capacity of 104 acre-feet.

Water quality basins, bio-swales or equally effective water quality measures will be used to satisfy the water quality requirements of



the Regional Water Quality Control Board and the City of Perris before the storm water runoff exits the Harvest Landing Site.

PVMDP Improvements

Several existing PVMDP facilities are located within Harvest Landing. These facilities will be designed to convey on-and off-site storm water to Line K and Line J. These facilities include Line K, Laterals K-16, K-17, K-18, K-19, K-20, K-21, K-22, K-23, K-24, and J-5. An Area Drainage Plan amendment modified the locations of these facilities, their drainage areas, and aligned with the layout of Harvest Landing.

Line H, Laterals H-10 and J-9.1, and portions of J-9 are considered in the drainage analysis for Harvest Landing but are not proposed to be improved as part of Harvest Landing.

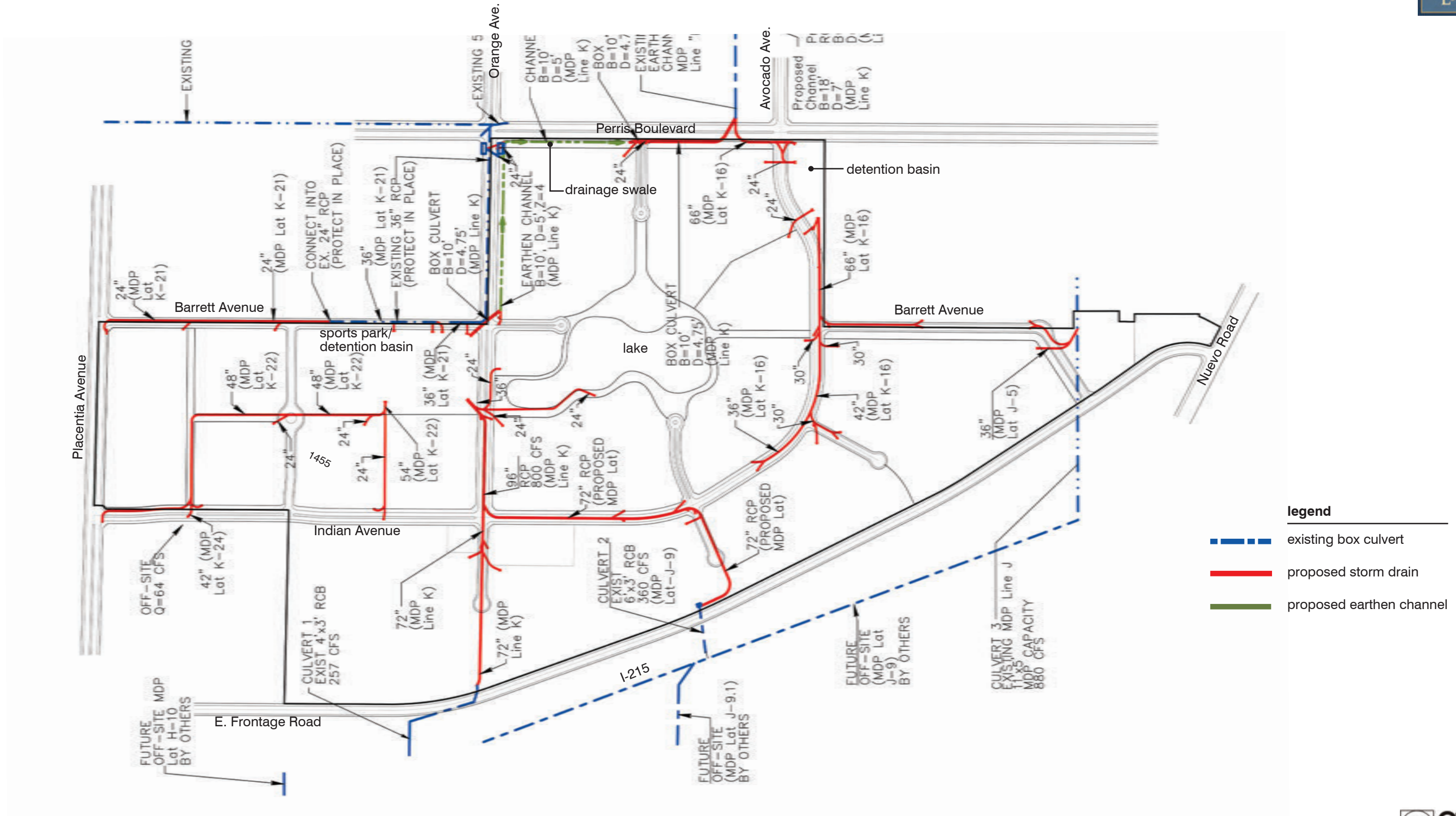
Prior to full improvement of the off-site PVMDP, it is necessary to protect Harvest Landing from off-site flows entering from the north. Accordingly, the preliminary grading and drainage systems assume an earthen berm along the northern boundary of the site to protect from these flows. Ultimately, these flows from the north will be collected by Line H and no longer drain onto Harvest Landing. At this time, the berm may be removed.

Drainage Standards

The development of the drainage system in Harvest Landing will be subject to the following standards:

- All drainage facilities will be designed and constructed in accordance with the Riverside County Flood Control and Water Conservation District (RCFC&WCD) standards and specifications, the City of Perris standards and the Standard Specifications for Public Works Construction.
- Drainage facilities will be subject to the review and approval of RCFC&WCD and the City of Perris.
- The Eastern Municipal Water District (EMWD) will review the design of drainage facilities in conjunction with their review of the sewer and water facilities.
- Drainage plans shall be submitted to EMWD for review and approval. This is to ensure that all proposed drainage facilities are compatible with the existing EMWD facilities.

- The capital cost of all on-site facilities will be the responsibility of the applicant. Such facilities will be dedicated to RCFC&WCD, the City of Perris, a Homeowners Association, or County Service area for maintenance and operations. There may be possible reimbursements from RCFC&WCD for the construction of Area Drainage Plan (ADP) facilities in the area. Applicable reimbursements are for ADP facilities larger than 36" in diameter.
- All areas within the Specific Plan area will be required to prepare a Water Quality Management Plan (WQMP) and a Storm Water Pollutant Prevention Plan (SWPPP) in accordance with the requirements of the National Pollutant Discharge Elimination System (NPDES) standards. Any industrial developments will require an additional SWPPP to operate.
- All projects proposing construction activities including: clearing, grading, or excavation that results in the disturbance of at least one acre total land area, or activity which is part of a larger common plan of development of one acre or greater shall obtain the appropriate NPDES construction permit and pay the appropriate fees. All development within the specific plan boundaries shall be subject to future requirements adopted by the County to implement the NPDES program. Mitigation measures may include, but not be limited to: on-site detention; water quality basins; covered storage of all outside facilities; vegetated swales; monitoring programs; etc.
- In the case of phased development, existing on-site drainage and drainage facilities that are outside of the improvement area will need to be accounted for. The developer's engineer shall provide solutions that address the drainage from any unimproved areas during the permitting process.



Note: Illustration is conceptual in nature and subject to change.



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Water Plan

The water plan for Harvest Landing is based on the availability of water and capacity of water systems surrounding the project as well as the maximum build-out of the project.

Water Supply

Harvest Landing lies within the service area of EMWD. To ensure an adequate water supply for the ultimate build out of Harvest Landing, a Water Supply Assessment dated December 20, 2006 was prepared by EMWD. In the Water Supply Assessment, it was estimated that the water demand for residential and non-residential use, not including irrigation, would be 825,000 gallons per day or 924 acre feet per year (AFY). The Water Supply Assessment also found that in the year 2010, EMWD's total water demand, including the build-out of Harvest Landing, would be 168,800 AFY. This is within EMWD's total water supply and the report concluded that there will be a sufficient water supply to allow for development of Harvest Landing.

EMWD also stated that the existing water facilities within the 1627 pressure zone, which contains Harvest Landing, are adequate to serve the project. Water in the 1627 pressure zone comes from a storage tank with a high waterline at an elevation of 1,627 feet above sea level. Since Harvest Landing will have buildings pad elevations ranging from 1,436 feet to 1,470 feet above sea level, there will be static pressure between 65 psi to 77 psi to provide sufficient water pressure to the site.

Water System Plan

As shown in Figure 3-22, *Water Plan*, water will be conveyed throughout Harvest Landing by a system of waterlines sized to meet ultimate plan buildout and to allow for adequate fire flow. To meet the projected demand, some existing waterlines will need to be upgraded and/or realigned. For instance, the existing 24" waterline in Indian Avenue will be realigned to the new roadway alignment and the existing 8" waterline in Orange Avenue will be upsized to a 12" waterline. The waterlines in Harvest Landing will connect to the existing lines in Perris Boulevard, Placentia Avenue, Indian Avenue, and Barrett Avenue.

Non-Potable Water System

Although EMWD concluded there is adequate water supply, Harvest Landing utilizes non-potable well water to further reduce water

demand. Non-potable water for Harvest Landing comes from two existing on-site wells, which will remain in place. The first well is located south of the intersection of Orange Avenue and Perris Boulevard and the second well is located north of the intersection of Arnold Avenue and Perris Boulevard. EMWD and the City of Perris have stated that the well water is an acceptable source for filling/maintaining the level of Harvest Lake and for irrigating public landscaping and parks.

The well water will be distributed throughout Harvest Landing by a separate system of 10" waterlines. This system will not tie into any other water system outside of Harvest Landing or into the potable water system.

The estimated water demand for the irrigation of landscaping/parks and the maintenance of Harvest Lake is approximately 480 AFY.² Based on available records, the two wells can accommodate the necessary non-potable water demand for Harvest Landing.

The development of the potable and non-potable water systems in Harvest Landing will be subject to the following standards:

- All water facilities will be designed and constructed in accordance with EMWD standards and specifications and the Standard Specifications for Public Works Construction (GREEN BOOK).
- Water facilities will be dedicated to EMWD for ownership, maintenance and operation. The capital costs for the facilities to serve this project (both on-site and off-site) will be the responsibility of the developer.

² Non-potable water demand includes the assumption that 40% of the multi-family areas, 20% of the multi-business areas, and 5% of the single-family areas will be irrigated.

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Sewer Plan

The Harvest Landing project lies within the service area of EMWD and the sewer plan for Harvest Landing is based on their regional master plan, the projected demand generated at project build-out, and the capacity of the sewer systems surrounding the project.

Of the total 341 acres in Harvest Landing, approximately 333.4 acres will sewer to the intersection of Orange Avenue and Perris Boulevard while the remaining 7.6 acres of the commercial site will tie into the existing sewer system and drain east in Nuevo Road.

As shown in Figure 3-23, *Sewer Plan*, both areas will sewer to the east and ultimately tie into an existing 42" trunk sewer line in Murrieta Road (capacity of 15.7 MGD), which is approximately one mile east of Harvest Landing. This 42" sewer line eventually drains to the Perris Valley Regional Water Reclamation Facility, which has an estimated capacity of 11 million gallons per day (MGD). In the future, this facility will be expanded to an overall capacity of approximately 22 MGD.

Some of the existing sewer facilities surrounding the project are not adequate to accommodate the build-out of Harvest Landing, which is projected to generate approximately 1.32 MGD of waste water. Consequently, the developers of Harvest Landing will be responsible for the following:

- 15" sewer in Perris Blvd between Avocado and Orange Avenues
- 15" sewer in Orange Avenue between Barrett Avenue and Perris Boulevard
- 8" to 12" sewer in Indian Avenue south of Orange Avenue

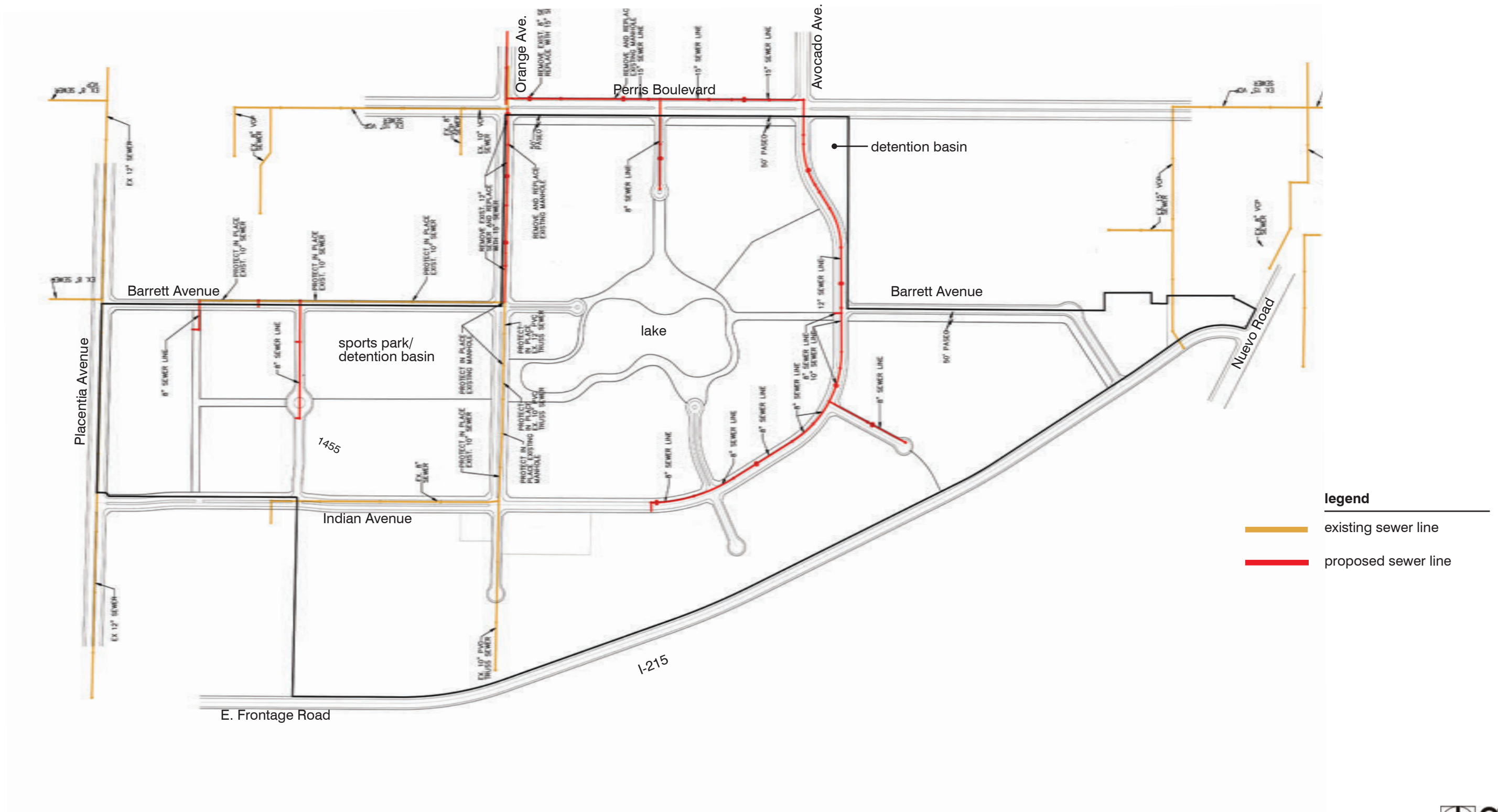
Table 3-9 describes the existing sewer lines impacted by Harvest Landing.

**Table 3-9
Existing Sewer Lines**

<i>Along (Street)</i>	<i>From (Street)</i>	<i>To (Street)</i>	<i>Size (In.)</i>	<i>Capacity (Gal/Min.)</i>	<i>Proposed Flow (Gal/Min.)</i>	<i>Replacement Size (In.)</i>	<i>New Capacity (Gal/Min.)</i>
Barrett Ave.	Orange Av.	S of Placentia Ave.	10	278	209	None	NA
Indian Ave.	Orange Avenue	N of Water St.	8	172	43	None	NA
Orange Ave.	W of Barrett Ave.	Frontage Rd.	10	278	261	None	NA
Orange Ave.	W of Barrett Ave.	Barrett Ave.	12	392	276	None	NA
Orange Ave.	Perris Blvd.	Barrett Ave.	12	392	436	15	915
Perris Blvd.	Orange Ave.	Arnold Ave.	8	172	492	15	915
Placentia Ave.	Perris Blvd.	Frontage Rd.	12	392	NA	None	NA

The development of the sewer system in Harvest Landing will be subject to the following standards:

- All sewer facilities will be designed and constructed in accordance with EMWD standards and specifications and the Standard Specifications for Public Works Construction (GREEN BOOK).
- Sewer facilities will be dedicated to EMWD for ownership, maintenance and operation. The capital costs for the facilities to serve this project (both on-site and off-site) will be the responsibility of the developer. EMWD will reimburse the developer for the upsizing costs related to the construction of the sewer line along Orange Avenue to Murrieta Road.



Note: Illustration is conceptual in nature and subject to change.



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Dry Utilities

Dry utilities including telecommunications, gas, electrical service, and solid waste collection will be available to Harvest Landing and will be extended into the planned community as part of the development.

Telecommunications/Cable

Verizon provides telephone service and Time Warner provides cable service within Harvest Landing.

Natural Gas

The Southern California Gas Company will provide natural gas service to Harvest Landing via an existing 6" gas main in Perris Boulevard.

Electricity

Southern California Edison (SCE) will provide electricity within Harvest Landing.

Solid Waste

Solid waste collection service in the City of Perris and Harvest Landing is provided by CR&R Disposal. Solid waste is first transported to Perris Materials Recovery Facility where recyclable materials are separated from solid wastes. Non-recyclable solid wastes are then transported to either the El Sobrante Landfill or to the Badlands Landfill in Moreno Valley.

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Section 4:

DESIGN GUIDELINES



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SECTION 4: DESIGN GUIDELINES

Design Guidelines are intended to create an attractive and cohesive community identity through the provision of detailed direction for the physical design of Harvest Landing. The guidelines below address design at three different levels: community, neighborhood, and building.

Harvest Landing is envisioned as a family of unique neighborhoods and activity areas. The intent of the Design Guidelines is to ensure that the multiple parts of the plan have a high level of quality to unify the project area. Architecture and amenities should be designed with timeless elements that will be appreciated as much in the future as when built. While promoting a high level of design quality, the Design Guidelines provide a wide degree of flexibility to encourage creativity on the part of property owners and designers.

The Design Guidelines are intended to be general and illustrative in nature. They are not rigid requirements; however, every development within Harvest Landing must incorporate the particular attention to detail reflected in these guidelines. These Design Guidelines establish the design framework that will be used to evaluate proposed developments. The intent of these guidelines must be met in order for a project to be approved during the development review process.

Community-Wide Design Guidelines

Community-wide design guidelines apply to the entirety of Harvest Landing. They are intended to create a strong community identity through the use of consistent design detail throughout the project area.

Design Theme

The community features and facilities (roadways, paseos, parks, entries, and the community recreation facility) of Harvest Landing should be designed in materials and styles that evoke the feeling of a lake front community. Doing so will strengthen the community's identity and provide residents with a unique living environment. Brick, stone, and wood-siding should be the primary materials used in the design of community entries and important community

facilities, such as the community recreation center. Figures 4-1 through 4-3 provide conceptual drawings of entries, roadways, and community features to illustrate the design theme of the Harvest Landing.

Streetscapes



Examples of the streetscape envisioned in Harvest Landing.

1. Streetscape elements, such as landscaping, lighting, street furniture, and signage should create an attractive, consistent, and cohesive community image.
2. Attractively landscaped medians should be provided along major roadways to reduce vehicular speed, create a pedestrian-friendly environment, and ensure a pleasant community image.
3. Special patterned paving should be provided at important intersections and pedestrian crossings within the Specific Plan area, such as the intersection of Barrett and Orange Avenues. Special intersection treatments should consist of materials such as tile, colored brick, or stamped concrete.
4. Streetscape elements, such as lighting, landscaping, and street furniture, should complement the surrounding architectural styles.
5. Traffic-calming measures, such as sidewalk bulbs, roundabouts, and chicanes, may be used in and adjacent to residential areas to reduce the speed of traffic and create a more pedestrian-friendly environment. Sidewalk bulbs increase pedestrian safety and comfort and should be placed at important intersections. If used, roundabouts should be placed in areas of high pedestrian movement to slow traffic and improve pedestrian safety.



Entries

Entries are critical opportunities to distinguish Harvest Landing from surrounding developments and announce arrival into the project itself. There are two types of gateways in Harvest Landing: Community and Secondary. As shown on Figure 4-1, *Entries*, Community Entries are located outside of the right-of-way at major intersections and entry points while Secondary Entries are located at minor entry points.

The treatments described below provide the desired quality of the two entry types. The exact design, configuration, and content of each gateway will be determined in detailed site plans at the tract map level.

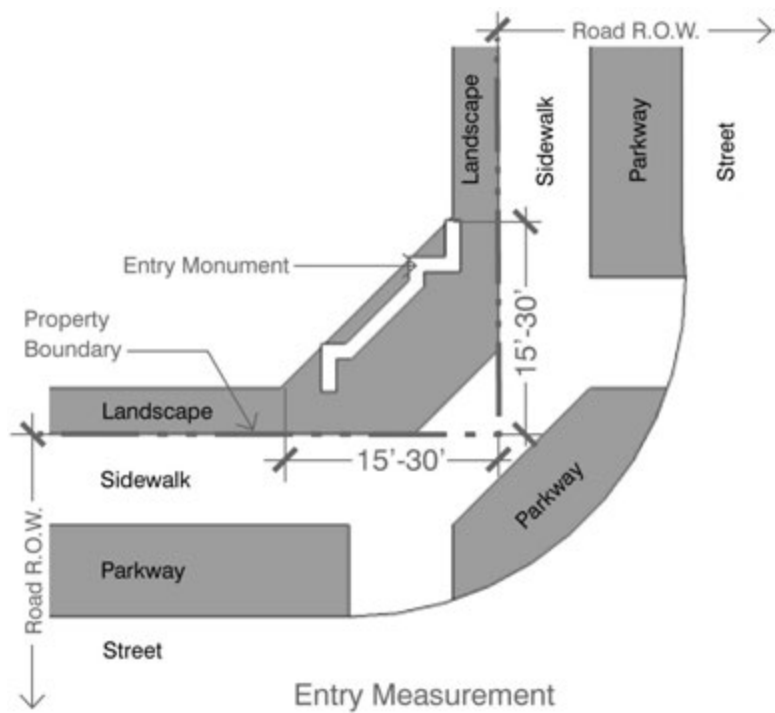


Entries announce arrival and distinguish Harvest Landing.

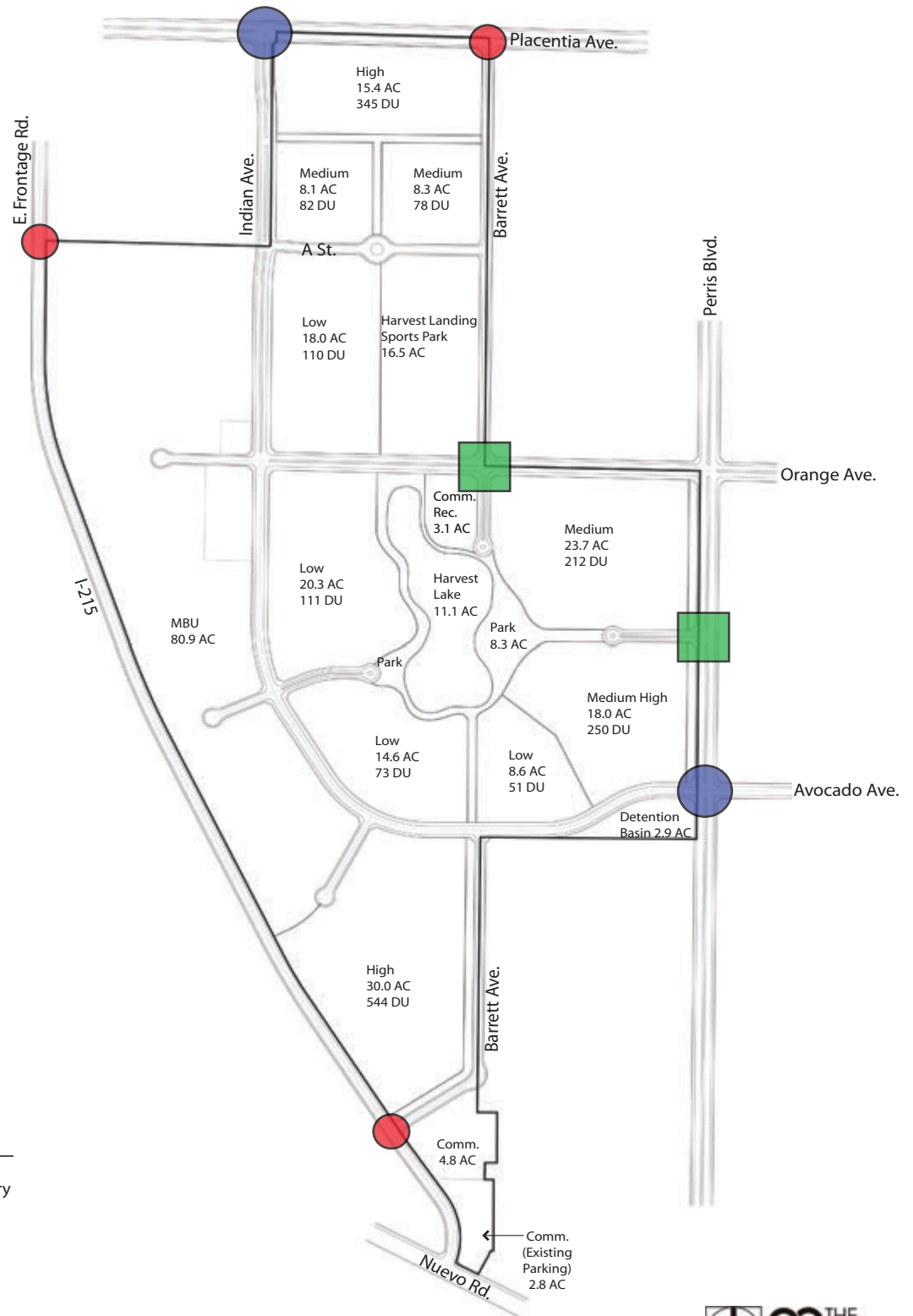
1. Community Entries are provided at: (1) Orange Avenue and Barrett Avenue, (2) Indian Avenue and Placentia Avenue, (3) Perris Boulevard and Indian Avenue, and (4) Perris Boulevard and the entry into the neighborhood surrounding Harvest Lake. Community Entries are the larger of the two entries. As measured from the right-of-way line, Community Entries typically range between 300 and 450 square feet with a dimension between 20 and 30 feet as measured from the corner of the property line. As shown on Figure 4-2, Community Entries should incorporate distinctive signage, attractive landscaping, special intersection treatments, and distinguishing elements such as monument style walls. Formal signature groupings of trees and landscaping can be incorporated into the design to create distinctive entries to the community.

The Community Entries at (1) Orange Avenue and Barrett Avenue and (2) Perris Boulevard and the entry into the neighborhood surrounding Harvest Lake are the key entries into the project. Accordingly, these two entries are endowed with greater emphasis in detail, as shown on Figure 4-3.

2. Secondary Entries typically range between 100 and 200 square feet with a dimension between 15 and 20 feet as measured from the street corner of the property line. As shown on Figure 4-4, Secondary Entries should be marked by attractive and consistent signage or landscaping and include elements similar to those found in the Community Entries, but smaller in scale.



ENTRIES



Entries Key

- Key Community Entry
- Community Entry
- Secondary Entry



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Note: Illustration is conceptual in nature and is subject to change. Entry design features to be determined at the time of detailed entry design.



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KEY COMMUNITY ENTRIES



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SECONDARY ENTRIES



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Pedestrian and Bicycle Connectivity

1. Pedestrian and bicycle routes along roadways and paseos should incorporate pedestrian amenities, such as benches, shade structures, and lighting.
2. Meandering trails and sidewalks, which are separated from the street by landscaping, are desirable.
3. Pedestrian connections should be created within residential neighborhoods, the multiple business use area, and the commercial area. These neighborhood-level connections should tie into the paseos and walkways to create a cohesive pedestrian and bicycle circulation system within the project.
4. Bike racks should be located at strategic points throughout the open space system, such as playgrounds, parks, and other recreational amenities, to encourage the use of non-vehicular circulation.
5. Trails and paseos should be clearly marked with consistent signage and well lit with bollard lighting as appropriate.



Create attractive and safe pedestrian connections in Harvest Landing.

Landscaping

1. All landscaping shall comply with Chapter 19.70 of the Municipal Zoning Code dated April 25, 2005. In addition, all landscaping shall utilize the approved trees, shrubs, and groundcovers listed in the Landscape Plant Palette, Appendix A, of this Specific Plan.
2. Landscaping along major roadways and at project entries should be tasteful and consistent to create an attractive and cohesive community identity. Formal plantings of non-native species may be used at key entries and intersections to highlight these areas.
3. Landscaping within paseos should consist of informal groupings of shade trees, shrubs, and plantings. Native and other low-water species should comprise the majority of landscaping within paseos. However, turf areas may be provided in areas intended for active use.



Landscaping plays a critical role in the character of a development and must be thoughtfully integrated into a community.

4. Median plantings should reflect the use of the roadway of which they are a part. Median plantings along Perris Boulevard, Placentia Avenue, Indian Avenue, and Orange Avenue should be more formal than those along project arterials, to reflect the hierarchy and character of the roadways.
5. Water usage should be minimized through the planting of native and low-water species and the utilization of water-efficient and drip irrigation systems.
6. As practical, utilize medians and parkways for water treatment and to reduce runoff.
7. Landscaping should be used to buffer adjacent land uses that are different in scale, use, or design.
8. As practical, ample street and shade trees should be provided to create an attractive and pedestrian friendly environment.
9. As appropriate, an arborculturalist should be consulted to ensure the implementation of proper arborcultural practices.
10. As practical, utilize a variety of tree types with an emphasis on trees that provide canopy cover and an urban forest within Harvest Landing.



Examples of, from top to bottom, perimeter walls, view fences, and screening walls that are envisioned for Harvest Landing.

Walls and Fences

1. Solid walls and fences should not dominate the street scene. They should only be used when necessary for noise attenuation, privacy, and shielding of incompatible adjacent uses.
2. View fences (which are partially solid and see-through material such as wrought iron) provide a visually attractive alternative to solid walls and fences. They allow for safety and privacy while preserving views and creating a more visually appealing neighborhood. View fences should be used instead of solid walls when feasible, especially around the lake and facing onto parks and paseos.
3. Wall faces that are visible to the public should be constructed of attractive materials and finished with architectural



detailing or articulation. The incorporation of high quality materials and surface articulation are strongly encouraged.

4. Walls and/or wall surfaces not visible to the public do not need the same high level of design detail. Pilasters should be incorporated into wall design, especially at entries and important community intersections.
5. Trees, vines, and landscaping should be used to soften the visual appearance of the walls. Intermittent undulation of walls will help to create a varied street scene and is strongly encouraged.
6. Walls and fences should be made of durable and yet attractive materials that complement the adjacent architecture and are resistant to graffiti. When solid walls are necessary, split-face block, stone, or materials with similar visual qualities should be used. Likewise, view fences should incorporate visually attractive materials such as tubular steel, wrought iron, and stone (or faux-stone).
7. Long, monotonous walls are to be avoided. Walls should be undulated with breaks, recesses, and offsets, especially at entries and important intersections. Long walls should be made more attractive and visually interesting through the incorporation of surface articulation and pilasters.

Neighborhood-Level Design Guidelines

Neighborhood-level design guidelines address the design of different neighborhoods within the Specific Plan area. The neighborhoods should be distinct areas within the project while retaining the necessary characteristics for overall community identity. The neighborhood-level design guidelines are divided into four categories: general, residential, multiple business use and commercial, and open space and recreation. General standards apply to all types of projects while the residential, multiple business use and commercial, and open space and recreation sections apply only to those specific land uses.

General

1. Attractive and consistent lighting elements should be provided along roadways within the neighborhood. The height,

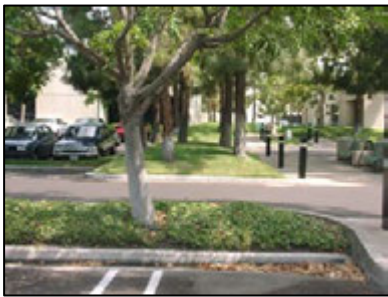


Use themed and pedestrian-level lighting in Harvest Landing.

brightness, and spacing of the lighting elements should be appropriate to the scale and speed of the roadway.



Externally lit signs are desirable in Harvest Landing.



Parking lots should incorporate screening, shade trees, and illuminated pedestrian walkways.



Example of landscaping used to screen and soften a long expanse of wall.

2. Pedestrian-scale lighting should be provided in areas with nighttime use, such as paseos and walkways. The type, style, and intensity of pedestrian lighting should reflect the use of the character of the area.
3. Since Harvest Landing is located in the Mount Palomar nighttime lighting sphere, lighting elements should minimize glare, spill over, and light pollution.
4. Lighting fixtures should be compatible with the architectural styles of surrounding buildings and yet consistent throughout the community.
5. Lighting standards should be consistent with City safety and illumination requirements.
6. Iconic landscaping and buildings within the project should be spotlighted to provide visual accent and directional reference.
7. Project signage should have a consistent design theme and color palette throughout the development to achieve a coordinated sense of identity.
8. Signage should be of a scale and style appropriate to the area and surrounding uses.
9. Business related signage may be interior illuminated and residentially related signage and entries should be externally illuminated.
10. Freestanding monument-style signage should be used. Pole signs are prohibited.
11. Signs should utilize materials and colors that are compatible with the architecture and designs of the surrounding areas.
12. Parking areas should be designed to minimize the conflict between pedestrian and vehicular traffic.
13. Parking areas should provide clearly marked and well-lit pedestrian walkways to ensure pedestrian safety.



14. Parking areas should be tucked behind buildings when possible. Doing so will minimize the visual impact of these areas along the roadway.
15. When it is necessary to place parking lots adjacent to the street, the parking area should incorporate design features to shield it from the roadway. Appropriate design features include berms, landscaping, trellises, and low, decorative walls.
16. Shade trees and structures should be incorporated into parking lot design.
17. Trees, shrubs, and vines soften the visual appearance of walls and are strongly encouraged.
18. Walls should provide convenient pedestrian gates to ensure ease of pedestrian circulation.
19. Utilities should be screened and/or located underground as appropriate.
20. Aboveground utilities shall be screened from public view.

Residential

1. Avoid monotony through the utilization of varied setbacks, garage orientation and location, and building massing.
2. Designs with the garage as a dominant feature along the street are strongly discouraged. The placement, orientation, and size of the garage should be varied and include a mixture of front, side, rear, and alley-loaded garages as feasible.
3. Alley-loaded products help to create a pedestrian-friendly streetscape and are encouraged.
4. Varied front and side setbacks create a more interesting street scene and are strongly encouraged.
5. Roadways should be oriented to create views of neighborhood parks and other amenities.
6. The creation of pocket parks and pedestrian trails and paths within neighborhoods is encouraged.



Use varied setbacks, rooflines, and architectural styles to create visual interest in a neighborhood.

7. Alleys should be landscaped to improve their aesthetic quality.
8. In multifamily developments, adjacent buildings should have varied setbacks, floor plans, color and material palettes, and architectural detailing.
9. In multifamily developments, the architectural building edge should be used in place of walls whenever feasible.
10. Architectural styles and detailing are extremely important to the identity and appeal of residential neighborhoods as well as individual homes. A mix of well-defined architectural styles in each neighborhood will add to the character of the project and is encouraged.
11. View fencing and decorative walls should be utilized where residential uses are adjacent to open space areas, such as parks and paseos. They allow for privacy without completely blocking views.
12. On-street parking should be provided on local and private residential streets when possible. On-street parking tends to slow traffic and serves as a barrier between vehicular traffic and pedestrians, creating a safer pedestrian environment.

Freeway Residential Edge Conditions

1. Maintain at least a 75 foot buffer between the freeway right-of-way and residential units. See Section 3, *Development Standards*, for information on freeway residential edge condition requirements.
2. As practical, locate parking areas and driveways between the freeway and residences to provide a wider buffer than required.
3. Orient buildings, doors, openings, windows, and common/private open spaces away from I-215 as practical.
4. As appropriate, utilize air filters and air conditioning units to minimize the impact of the freeway on interior air quality.
5. As necessary, utilize double paned windows to reduce noise impact on residents.
6. Utilize tall landscaping for screening purposes.



Multiple Business Use and Commercial

1. The multiple business use and commercial areas should present a clean and attractive professional appearance.
2. When possible, adjacent buildings should be designed with shared open spaces that can serve as employee or customer outdoor eating and gathering places.
3. Projects within the multiple business use and commercial areas should utilize specialized design features, such as distinctive architectural detailing, landscaping, and lighting elements, to ensure a consistent design theme for that project.
4. Structures should be designed to relate to each other and the surrounding area.
5. A variety of paving materials should be used to clearly delineate vehicular and pedestrian areas and to create a sense of place.
6. Entries to large complexes or large individual structures within the multiple business use and commercial areas should be clearly marked, with distinctive landscaping and attractive monument-style signage.
7. Storage, loading areas, and service entrances should be located to the rear of the structure and oriented away from noise sensitive uses, such as residences. Landscaping, berming, and walls should be used to buffer adjacent uses from these areas.
8. Accessory uses and utilities should be located behind the primary structure out of public view whenever possible. Uses that cannot be placed out of view should be shielded with berms, landscaping, attractive walls, or decorative screening.
9. Signage should be visually interesting and informative. It should be distinctive and eye-catching yet simple and tasteful. Cluttered designs and excess advertising should be avoided.
10. Multitenant signage is encouraged to minimize visual clutter.

11. The design and style of a sign should complement the architectural style of the building to which it is attached or adjacent.
12. When feasible, parking areas should be designed so that they can be shared between uses.
13. Access should be located as far from street intersections as possible and taken from the lowest classification street practical.
14. Disperse parking into multiple smaller lots as opposed to one large lot where feasible and screen parking with berms, landscaping, or fences.

Recreation Areas and Open Spaces



Given the climate of Perris, use landscaping to shade recreation areas.

1. Recreation and open space areas should be designed to accommodate the needs of different ages and abilities.
2. Canopy trees should be used to provide shade. Informal groupings create visual interest and are encouraged.
3. Ample outdoor furniture should be provided. This furniture should match the surrounding architectural styles, materials, and colors. Benches should be placed to take advantage of views, especially around the lake.
4. A combination of hard and soft paving may be used depending upon the function of the recreational amenity.
5. Active areas may utilize turf, grasses, and ornamental plantings. Passive areas should primarily be composed of nonirrigated or drought-tolerant species.

Building-Level Design Guidelines

Building-level design guidelines provide important design criteria for structures within Harvest Landing. These guidelines address building-level design in three categories: general, residential, and multiple business use and commercial.



General

1. Orient buildings to face onto streets, parks, and open spaces/paseos. This orientation will create more attractive, safe, and pedestrian-friendly streetscapes and public spaces.
2. Vary rooflines and building heights to create visual interest.
3. Incorporate significant architectural features, such as towers or balconies, into the design of key community buildings, such as the community center. Doing so will strengthen the identity of the project.
4. Varied massing and vertical step-back in building design prevents a monotonous streetscape and is encouraged.
5. Architectural detailing and surface articulation is encouraged to be applied equally to all sides of a structure but is mandatory on all elevations that are visible to the public.
6. Quality materials should be used in the construction of buildings.
7. Variations in style, architectural features, and colors create visual interest and are encouraged.
8. Accessory structures, such as storage areas, trash receptacles, mechanical equipment, parking structures, backflow preventers, loading docks, security fences, and similar uses can seriously detract from the visual quality of an area. Therefore, care must be taken to minimize the visual impact of these uses through site design and visual shielding. When possible, these uses should be located away from roadways and public views, behind buildings, or in enclosed structures. Effective shielding methods include landscaping, berms, walls and fences, and ornamental screening.
9. Accessory structures should be designed to look like a continuation or extension of the primary structure. They should have architectural detailing and landscaping similar to that of the primary structure.



Vary rooflines, distinguish corners with unique architectural elements, and orient buildings toward open spaces to create a unique living environment.



Examples of products oriented toward open space, the use of quality building materials, and incorporation of front

Residential



Vary roof forms, materials, and colors as well as entry features to add variety to a neighborhood.



Example of varied use of material, corner treatment, and articulated facades, which add interest to a building.

1. Box-like designs are prohibited. Varied architectural detailing and projections should be used to accentuate specific features and ensure a visually pleasing and varied experience within a neighborhood.
2. Porches, detailed entries, and stoops add to the character of a neighborhood and are encouraged. The majority of units should incorporate a front entry feature into their design. These features should be varied along the street to create visual interest. If possible, these features should project forward of a front entry garage door.
3. Variation in floor plans, unit types, roof form, colors, and materials adds character and visual interest to a neighborhood and should be incorporated into a project's design.
4. Entry features, such as gates, trellises, arches, and arbors should be employed to add visual interest and variety within the neighborhood.
5. Buildings on corner lots set the tone for the neighborhood. Therefore, these buildings should be subject to special design attention and detailing on both street-facing sides.
6. Utilize a variety of architectural features and detailing methods to differentiate adjacent structures. Features such as articulated entry features (porches and stoops), bay windows, and color and material variations are encouraged.
7. Front-entry garage doors should not be the dominant feature of a façade. They should be recessed into the garage wall and should include patterns, modulation, or indentations to create visual interest. Decorative panels, windows, arched doorways, and ornamental trim are strongly encouraged.
8. Large multiple-family dwellings should have articulated facades, including recesses and architectural detailing, to avoid a monotonous streetscape.



Multiple Business Use and Commercial

1. Avoid long, monotonous building façades and create diversity by clustering buildings around courtyards and open areas.
2. Establish a visual link in multi-building complexes by using architectural and site design elements to unify the project.
3. Building entries should be oriented towards the street and clearly defined.
4. Avoid a single, large dominant building mass. Horizontal building masses shall not exceed a height-to-width ratio of 1:3 without substantial changes in height and projecting or recessed elements.
5. Architectural elevations of principal buildings over 20 feet tall may have a clearly discernable base (approximately 3 feet), body, and cap (consisting of a cornice, parapet, awning, or eave that do not to exceed the base height). If used, the base and cap should be discernable from the body by changes in color, materials, texture, pattern, or profile.
6. Utilize high quality materials on building bases, bodies, and caps as described on the following list of permitted materials:
 - a. Building base—Brick, native stone, manufactured stone, or decorative concrete masonry units.
 - b. Building body—Wood, brick, native or manufactured stone, concrete, glass, stucco, or exterior insulated finish systems. Imitation wood siding, sheet metal, corrugated metal, or other similar metal panels are considered inappropriate and should be avoided. Reflective or highly tinted glass should be avoided.
 - c. Building cap—Utilize materials introduced in and complementary to the base and/or body of the building.
7. Utilize low reflective, subtle, neutral, or earth-tone colors on the building body. Building trim and accent areas may feature brighter colors, including primary colors. Applied paint over brick and stone is strongly discouraged.



Example of a well-designed project with unified architectural theme, unique corner treatment, coordinated signage, and clean landscaping.



Illustration of the base, body, and cap that may be used as architectural features in some building styles.



Examples of articulated building façades, interesting building masses, varied rooflines, and appropriate use of colors and materials.

8. Outdoor storage areas and loading docks should be screened from the street and adjacent uses.
9. Large buildings should have articulated façades, including recesses and architectural detailing, to avoid a monotonous streetscape. Openings should be recessed 2 to 4 inches to further articulate the façade.
10. Vary rooflines to reduce the overall mass of the building. Parapet walls and roof systems shall be designed to conceal all roof-mounted equipment from adjacent properties and public rights-of-way. Flat roofs should be disguised through the use of parapet walls.
11. Signs should be backlit or downlit to prevent glare and spillover onto adjacent properties.
12. “Canned” signs are prohibited. Interior illuminated channel letters should be used instead.
13. Signs should be compatible with the building and site design in terms of color, material, and placement.
14. All signs shall be maintained in good repair.
15. Color schemes for signage shall be related to other signs and graphics in the vicinity to achieve a coordinated sense of identity.
16. The exposed backs and sides of signs shall be architecturally treated to blend with the exterior character of the buildings and site.




Examples of appropriate interior illuminate, channel signs that are compatible with the building in terms of color, scale, and placement.



Menu of Architectural Styles

The following menu describes the architectural styles that are likely to be developed in Harvest Landing. The menu of architectural styles is not an exclusive list and other acceptable styles may be approved through the design review process with an accompanying description of style elements.


TUSCAN	
Style Elements	
Form	<ul style="list-style-type: none"> ▪ Informal floor plans and building massing ▪ Simple box-like forms with various plate heights ▪ Occasional use of arcades and colonnades
Roof	<ul style="list-style-type: none"> ▪ Shallow pitched gabled or hip roofs 3½:12 to 5:12 ▪ Large overhangs of up to 24" with exposed beam ends and soffits ▪ Variegated clay or concrete tile in terra cotta and earth tones
Walls	<ul style="list-style-type: none"> ▪ Smooth stucco finish ▪ Occasional use of brick ▪ Randomly laid stone with horizontal bonding courses
Windows	<ul style="list-style-type: none"> ▪ Small single-casement windows ▪ Special groupings of casements for prominent accent windows ▪ Arched openings for large expanses of glass ▪ Occasional use of horizontal stone lintels
Colors	<ul style="list-style-type: none"> ▪ Subdued earth tones ▪ Occasional use of brighter earth tones; terra cotta, ochre, and peach ▪ Accented window surrounds with subtle color contrast
Details	<ul style="list-style-type: none"> ▪ Accent shutters ▪ Awnings ▪ Trellis and pergolas ▪ Wrought iron balconettes ▪ Window flower boxes
Typical Elevation	

COTTAGE	
Style Elements	
Form	<ul style="list-style-type: none"> ▪ Gentle to steep pitched roofs with projecting gable ends ▪ Typically steep second-story roofs breaking over first-story elements ▪ Gable, hip, and/or shed roof dormers
Roof	<ul style="list-style-type: none"> ▪ 6:12 to 12:12 high pitch roof ▪ Flat concrete tile ▪ Tight to 12" overhangs ▪ Multiple gable element roofs with broken pitches
Walls	<ul style="list-style-type: none"> ▪ Stone or brick veneer base ▪ 1½- to 2-story stone or brick accent wall planes at front elevation ▪ Sand stucco or smooth finish ▪ Siding accents in gable ends
Windows	<ul style="list-style-type: none"> ▪ Bay windows ▪ Rectangular multipaned windows ▪ Round and curved top windows ▪ Simplified windows on side and rear elevations
Colors	<ul style="list-style-type: none"> ▪ Light earth-tones ▪ Contrasting cool or warm trims ▪ Whites or dark stucco body colors
Details	<ul style="list-style-type: none"> ▪ Shutters and recessed windows ▪ Decorative pot shelves ▪ Battered columns ▪ Entry door porches ▪ Wood columns with braces
Typical Elevation	



MONTEREY	
Style Elements	
Form	<ul style="list-style-type: none"> ▪ Single-gable roof side-to-side or back-to-back ▪ 2-story horizontal massing, simple form ▪ Second-story balcony at front elevations ▪ Vertical massing held to 25% to 35% of front elevation ▪ Horizontal and vertical massing breaks
Roof	<ul style="list-style-type: none"> ▪ 5:12 to 8:12 normal main roof pitch ▪ Balcony shed roof break at 3½ to 4½ slope ▪ Shingle texture flat tile or 's' concrete roof tile with variegated or blended colors ▪ Overhangs with continuous fascia board and exposed rafters
Walls	<ul style="list-style-type: none"> ▪ Smooth stucco or sand finish ▪ Horizontal or vertical siding at 2nd floor balcony wall ▪ Horizontal or vertical siding accents at gable ends
Windows	<ul style="list-style-type: none"> ▪ Vertical window shape with multiple panes often used in groupings ▪ Gothic shape or curved top feature window at first floor ▪ Colonial window and door trim ▪ Simplified windows on side and rear elevations
Colors	<ul style="list-style-type: none"> ▪ White-tones or medium-range earth tones and white-toned trims
Details	<ul style="list-style-type: none"> ▪ Wood-framed balcony with knee-brace posts and simple wood column detail ▪ 4" to 6" diameter round gable tile vents ▪ Arched or sloping fin walls ▪ Recessed or shuttered windows ▪ Brick veneer wainscot at base of building
Typical Elevation	

FARMHOUSE

FARMHOUSE	
Style Elements	
Form	<ul style="list-style-type: none"> ▪ Variable pitched roofs with moderate overhangs ▪ Expressive structural elements such as rafters, brackets and columns ▪ Variable window types ▪ A mixture of materials such as stone and wood siding ▪ Porches incorporating heavy timber columns with knee braces.
Roof	<ul style="list-style-type: none"> ▪ Roofs gabled with a pitch range from 3:12 to 9:12 for the main roof ▪ Roofs with simple gable, hipped or gambrel forms, often with dormers ▪ Roof materials to be 40-year composition asphalt shingles, concrete tile or clay tile
Walls	<ul style="list-style-type: none"> ▪ Walls clad with stucco or wood siding
Windows	<ul style="list-style-type: none"> ▪ Windows broad in proportion ▪ Windows have trim details over the window head ▪ Box and angled bay windows used as accents ▪ Picture windows ▪ Windows double hung with multiple pane over one division
Colors	<ul style="list-style-type: none"> ▪ Colors reflective of and complementary to materials used for walls, such as wood siding ▪ Stone and brick used as accents
Details	<ul style="list-style-type: none"> ▪ Simple eave projections with trimmed rafter tails and fascia ▪ Broad porches, encompassing the full width of the house ▪ Piers and balustrades of the same material as the main mass of the house ▪ Porch balustrades are connected to column supports
Typical Elevation	



CRAFTSMAN	
<i>Style Elements</i>	
Form	<ul style="list-style-type: none"> ▪ Cross-gabled or gable-fronted one or two-story box-like massing under main roof ▪ Entry porch
Roof	<ul style="list-style-type: none"> ▪ 4:12 to 8:12 normal roof pitch ▪ Shingle texture flat concrete roof tile ▪ Plain cut rafter tails ▪ Overhangs 6" to 18" ▪ Hip and gable roofs
Walls	<ul style="list-style-type: none"> ▪ Horizontal siding ▪ Plain wood shingle siding ▪ Sand or smooth stucco finish ▪ Blended stucco and siding ▪ Stucco with stone base, battered wall forms
Windows	<ul style="list-style-type: none"> ▪ Ribbon windows – 3 or more ganged ▪ Single-hung windows ▪ Mullion upper half of window ▪ Simplified windows on side and rear elevations
Colors	<ul style="list-style-type: none"> ▪ Light or deep earth-tone colors
Details	<ul style="list-style-type: none"> ▪ Stone and brick blended accent chimney ▪ Full length porch ▪ Layered wood entry door and window details ▪ Classically styled columns (i.e., Battered or Heavy Square) ▪ Decorative ridge beams and purlins ▪ Stone or brick base ▪ Triangular knee braces
Typical Elevation	

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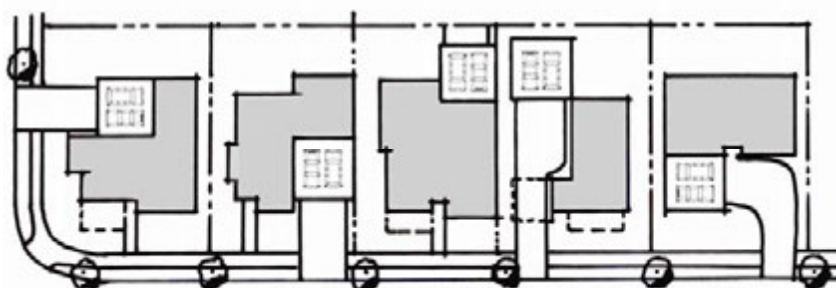


Residential Home Types


A variety of home types will create an interesting community and provide for a wide range of homebuyers and lifestyles. The following residential home types are examples of housing products that could be developed in Harvest Landing. This is not an exclusive list of home types and other products may be proposed. The following examples or home types are intended to provide the reader with an understanding of current (as of 2007) products that are typical of each density range.

Single Family Detached	
Style Elements	
Characteristics	<ul style="list-style-type: none"> ▪ Homes are oriented to face the street. ▪ Alternating architectural styles, colors, and garage orientations create a more interesting streetscape.
Garage Orientation	<ul style="list-style-type: none"> ▪ Front loaded or alley loaded
Alleys	<ul style="list-style-type: none"> ▪ Sometimes
Front Door Orientation	<ul style="list-style-type: none"> ▪ Street facing
Land Use Categories	<ul style="list-style-type: none"> ▪ Low Density Residential and Medium Density Residential
Typical Photo	

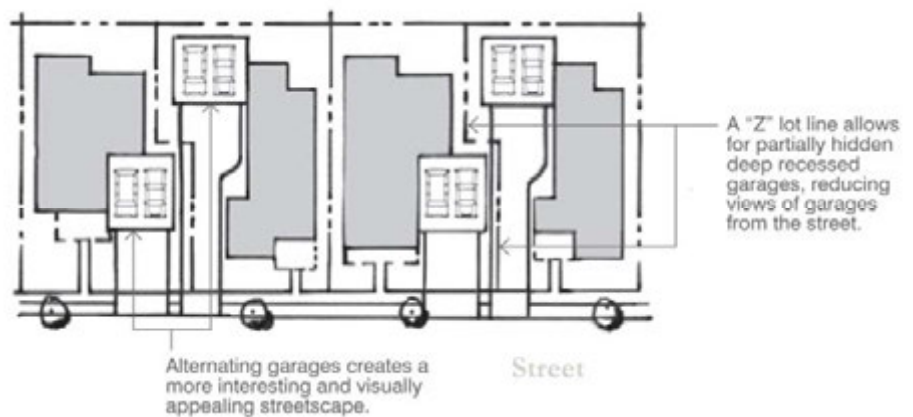
Typical Site Plan



2-Pack Single Family Detached

Style Elements	
Characteristics	<ul style="list-style-type: none"> ▪ A "Z" lot line allows for alternating garages to be deeply recessed, decreasing the visual effect of garages on the streetscape. ▪ Garages are alternatively located in the front and rear portions of the lot, creating variation along the street front.
Garage Orientation	<ul style="list-style-type: none"> ▪ Front loaded
Alleys	<ul style="list-style-type: none"> ▪ No
Front Door Orientation	<ul style="list-style-type: none"> ▪ Street facing
Land Use Categories	<ul style="list-style-type: none"> ▪ Low Density Residential and Medium Density Residential
Typical Photo	

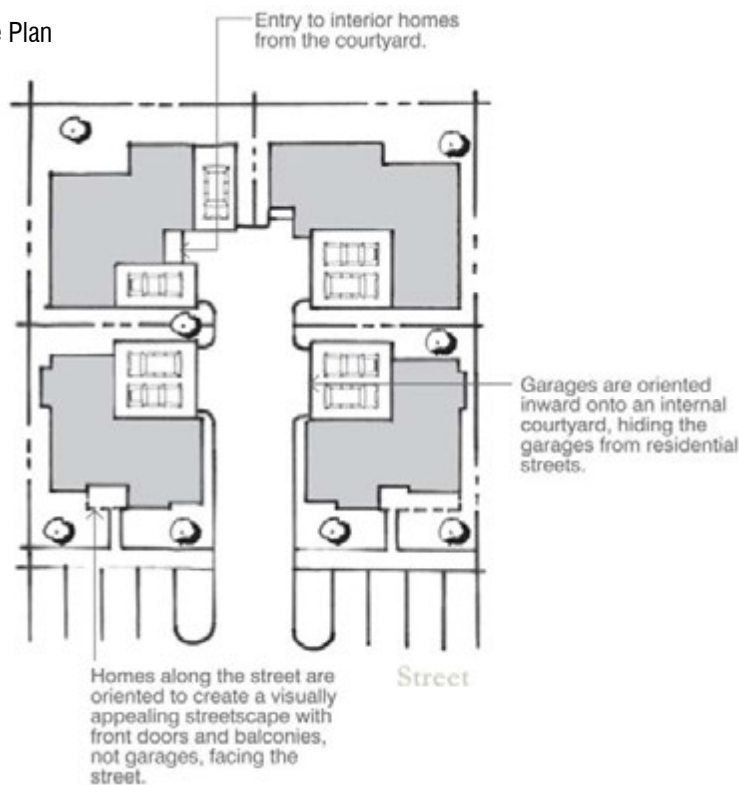
Typical Site Plan






4-Pack Courtyard Single Family Detached	
Style Elements	
Characteristics	<ul style="list-style-type: none"> Four single-family detached homes clustered around an interior courtyard
Garage Orientation	<ul style="list-style-type: none"> Onto interior courtyard
Alleys	<ul style="list-style-type: none"> No
Front Door Orientation	<ul style="list-style-type: none"> Street facing for homes along street Into interior courtyard or onto greenbelts/paseos/parks for interior homes
Land Use Categories	<ul style="list-style-type: none"> Medium Density Residential
Typical Photo	

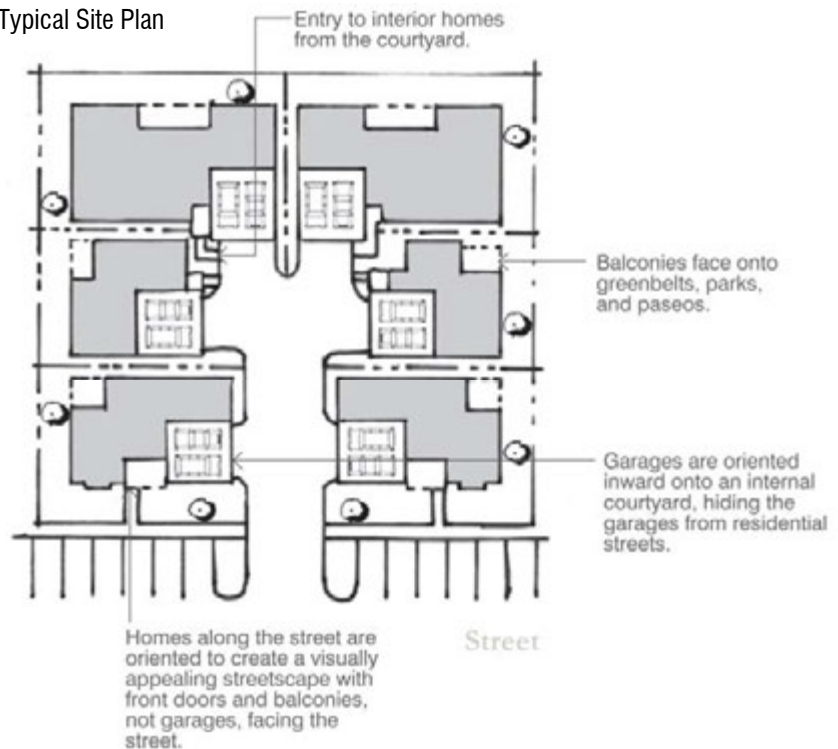
Typical Site Plan



6-Pack Courtyard Single Family Detached

Style Elements	
Characteristics	<ul style="list-style-type: none"> ▪ Six single-family detached homes clustered around an interior courtyard
Garage Orientation	<ul style="list-style-type: none"> ▪ Onto interior courtyard
Alleys	<ul style="list-style-type: none"> ▪ No
Front Door Orientation	<ul style="list-style-type: none"> ▪ Street facing for homes along street ▪ Into interior courtyard or onto greenbelts/paseos/parks for interior homes
Land Use Categories	<ul style="list-style-type: none"> ▪ Medium Density Residential
Typical Photo	

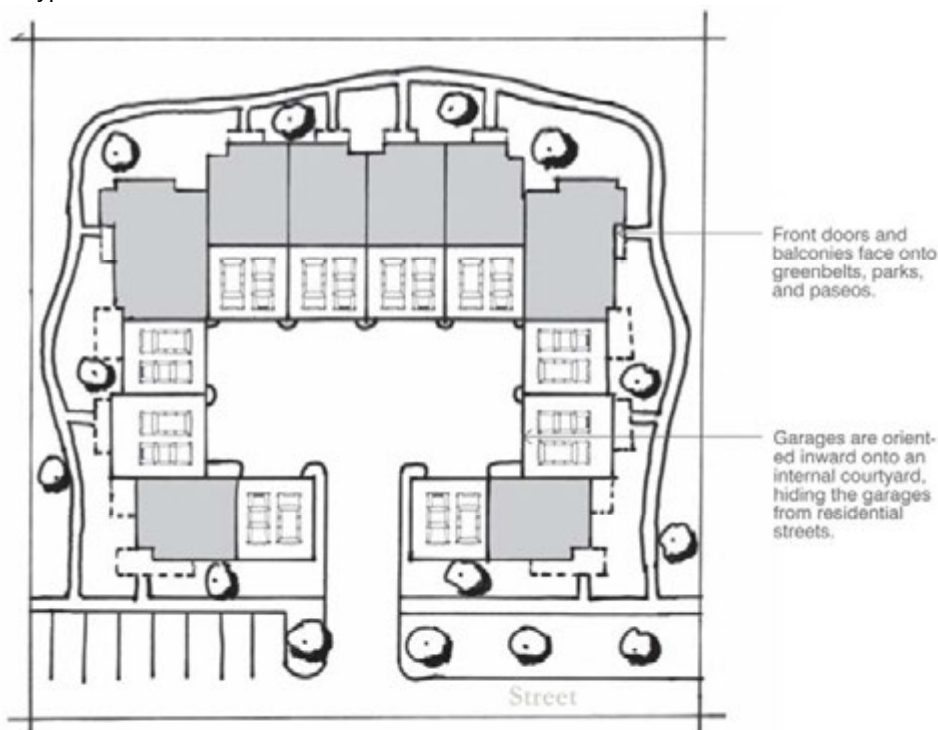
Typical Site Plan






Motorcourt Townhome	
Style Elements	
Characteristics	<ul style="list-style-type: none"> ▪ Townhomes clustered around a central courtyard ▪ Homes face onto greenbelts, paseos, or parks
Garage Orientation	<ul style="list-style-type: none"> ▪ Onto interior courtyard
Alleys	<ul style="list-style-type: none"> ▪ No
Front Door Orientation	<ul style="list-style-type: none"> ▪ Street facing for homes along street ▪ Onto greenbelts/paseos/parks for interior homes
Land Use Categories	<ul style="list-style-type: none"> ▪ Medium Density Residential, Medium High Density Residential, and High Density Residential
Typical Photo	

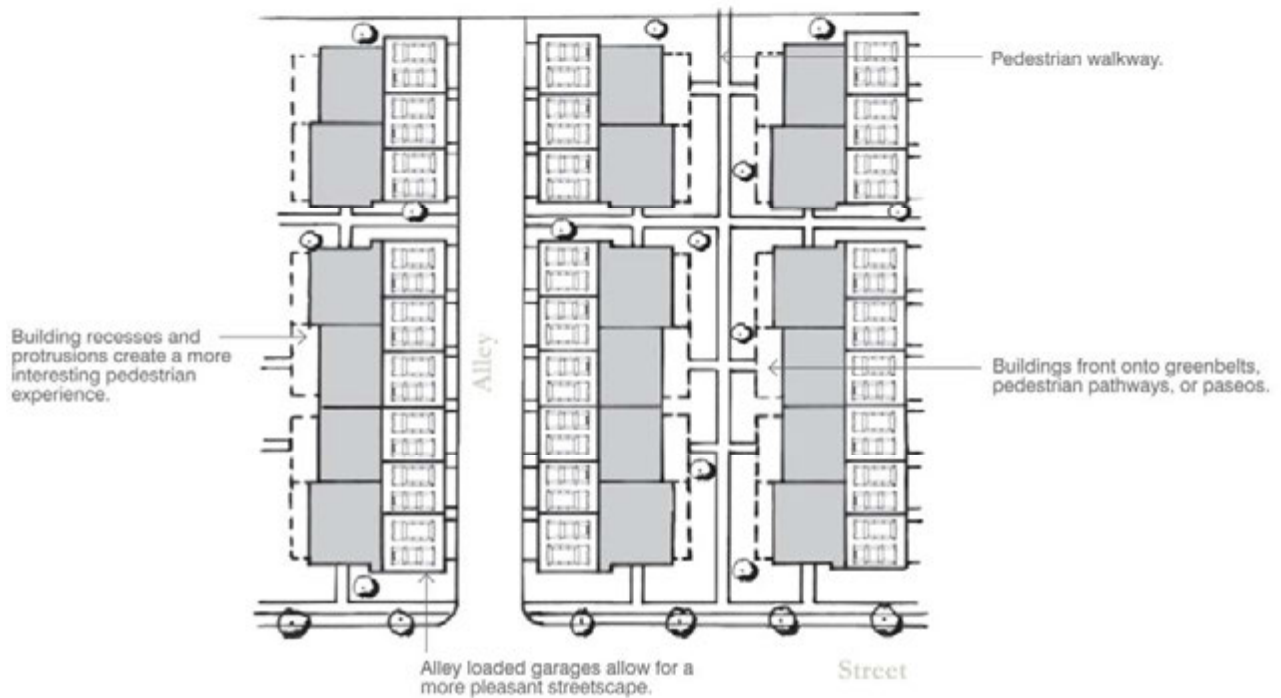
Typical Site Plan



Row Townhome

Style Elements	
Characteristics	<ul style="list-style-type: none"> Townhomes lined along alleys fronting onto a greenbelt or paseo
Garage Orientation	<ul style="list-style-type: none"> Onto alley
Alleys	<ul style="list-style-type: none"> Yes
Front Door Orientation	<ul style="list-style-type: none"> Street facing for homes along street Onto greenbelts/paseos/parks for interior homes
Land Use Categories	<ul style="list-style-type: none"> Medium Density Residential, Medium High Density Residential, and High Density Residential
Typical Photo	

Typical Site Plan





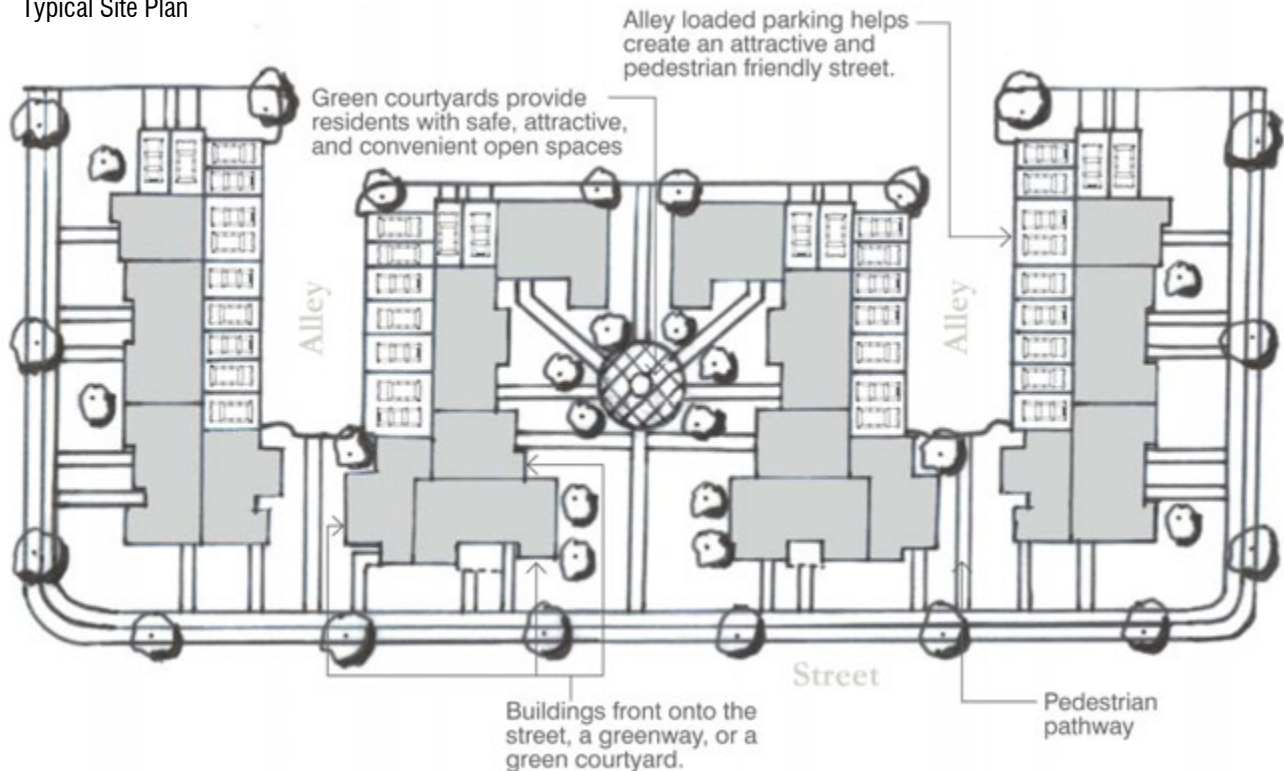
Greencourt Townhome

Style Elements	
Characteristics	<ul style="list-style-type: none"> Townhomes clustered around a central courtyard Homes face onto greenbelts, paseos, or parks Often separated by green pedestrian pathways/paseos
Garage Orientation	<ul style="list-style-type: none"> Onto interior alley
Alleys	<ul style="list-style-type: none"> Yes
Front Door Orientation	<ul style="list-style-type: none"> Street facing for homes along street Onto greenbelts/paseos/parks for interior homes
Land Use Categories	<ul style="list-style-type: none"> Medium Density Residential, Medium High Density Residential, and High Density Residential

Typical Photo



Typical Site Plan



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Section 5:

IMPLEMENTATION



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SECTION 5: IMPLEMENTATION

Whether regulatory or policy oriented, all specific plans must contain a “program of implementation measures, including regulations, programs, public works projects, and financing measures” pursuant to California Government Code, Section 65451(a)(4).

Other documents have been prepared and processed concurrently with the adoption of the Harvest Landing Specific Plan, including a General Plan amendment and a certified environmental impact report (EIR). These documents will form the basic framework to guide future development within the Specific Plan area.

Administration

Responsibility

The City’s Director of Development Services shall be responsible for administering the provisions of the Harvest Landing Specific Plan in accordance with the provisions of this Specific Plan, the State of California Government Code, Subdivision Map Act, and the City of Perris General Plan and Municipal Code.

Applicability

All development within the Specific Plan area shall comply with the requirements and standards set forth in this document. Where conflicts exist between the following standards and those found in the City of Perris Municipal Zoning Code, the standards contained in this document shall apply. The provisions of the City of Perris Municipal Zoning Code shall apply to any area of site development, administration, review procedures, environmental review, landscaping requirements, and parking regulations not expressly addressed by this Specific Plan document.

Interpretations

When there is a question or ambiguity regarding the interpretation of any provision of this Specific Plan, the Director of Development Services has the authority to interpret the intent of such provision.

The Director of Development Services may, at his/her discretion, refer interpretations to the Planning Commission for its consi-

deration and action. Such a referral shall be accompanied by a written analysis of issues related to the interpretation. The Planning Commission action may be appealed to the City Council.

All interpretations made by the Director of Development Services may be appealed to the Planning Commission in accordance with the appeal procedures set forth in the City's Municipal Code.

Specific Plan Amendment

Minor Amendments

Minor amendments include simple modifications to text or graphics that do not change the meaning or intent and are not contrary to any provision of the Specific Plan. This includes changes to Table 3-2, *Permitted Uses*, which facilitate/streamline the approval of uses that are not listed but similar in nature and impact to listed uses. Minor modifications may be accomplished administratively by the Director of Development Services and are appealable to the Planning Commission and City Council.

Changes in the land use plan that are in direct response to the ultimate MCP alignment shall be handled administratively and shall be considered minor amendments.

Major Amendments

Major modifications are amendments to exhibits or text that are intended to change the intent and/or development standards or other provisions of the Specific Plan. Major modifications require an amendment to the Specific Plan and approval by the Planning Commission and City Council in accordance with the requirements of the City of Perris Municipal Code.

Design Review

Attention to detail and design quality is a fundamental aspect of Harvest Landing. To ensure the desired quality, the following internal design review process is established.

Harvest Landing Design Review Committee

All applicable development proposals in Harvest Landing are required to be reviewed and approved by the Harvest Landing Design Review Committee prior to submittal of an application to the City. The Harvest Landing Design Review Committee shall be established by the Master Developer and may include builders,



architects, landscape architects, and others in the building and design profession.

The Harvest Landing Design Review Committee shall review applicable projects for consistency with the Specific Plan design guidelines with the intent of ensuring that each new development proposal conveys the desired character and quality and complies with the standards of the Specific Plan, including: communitywide, neighborhood level, and building level design, subdivision layout, pedestrian connectivity, landscaping, architectural style and variation, materials and colors, and signage.

The following development proposals are subject to review by the Harvest Landing Design Review Committee:

- New residential subdivisions, tract maps, site plans, and architectural plans (floor plans, elevations, landscape plans).
- New nonresidential subdivisions, tract maps, site plans, and architectural plans (floor plans, elevations, landscape plans).
- Initial signage programs (wall mounted and freestanding signs).

Subsequent structural and façade changes, building additions, and sign changes shall be reviewed and approved by the City of Perris.

Design Review Process

Step One—Conceptual Design Review Meeting. This is an informal meeting and an opportunity for the builder and architect to present initial design sketch ideas and concepts. At this meeting, the Harvest Landing Design Review Committee will review and provide feedback on the design direction of each proposal.

Step One Submittal Requirements

1. Provide fully dimensioned ¼" scale floor plans and elevations for each style/product proposed.
2. Show each floor plan on a lot indicating front, side, and rear setbacks.
3. Show landscape concepts for each floor plan on the site plan.
4. Provide a draft signage program if appropriate.

Step Two—Design Development Meeting. This is the project’s final design review meeting prior to submittal to the City. In this meeting, the builder team presents the final design package to the Harvest Landing Design Review Committee, who will review the drawings and discuss how the design has changed to respond to initial input.

The Harvest Landing Design Review Committee will respond within three working days and either approve the project to proceed to City submittals or provide suggestions for additional revisions.

Step Two Submittal Requirements

1. All site plans shall be prepared by a California registered architect and landscape plans shall be prepared by a California registered landscape architect.
2. Provide fully dimensioned ¼" scale floor plans and elevations for each style/product proposed.
3. Provide a final site plan showing front, side, and rear setbacks from the property line.
4. Provide all plans and addendum elevations for corner lots or special conditions clearly labeled as such.
5. Provide a landscape plan for each concept.
6. Provide a signage plan for each concept if appropriate.

Step Three—City Submittal. Upon approval by the Harvest Landing Design Review Committee, a letter of transmittal will be prepared by the Committee. The letter of transmittal will accompany all submittals to the City and will document that the project has met the internal design review process, complies with the design guidelines of the Specific Plan, and adheres to the desired character and quality of Harvest Landing.

Severability

If any section, subsection, sentence, clause, or phrase of this Specific Plan, or future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this plan.



Location of Land Use Designations

The locations of the land use designations are approximate and generally follow streets and property lines. Minor changes in boundary alignment and location are permissible with approval by the Director of Development Services. However, the intended character and overall location of the land use categories must be maintained.

Density Transfer

The ability to transfer density provides flexibility to respond to site constraints and market demands yet ensures that the overall goals of the Harvest Landing Specific Plan are maintained. Residential density transfer is allowable within the Specific Plan subject to the following criteria:

1. Density may not be transferred into any nonresidential land use category (e.g., Multiple Business Use, Harvest Lake, and Harvest Lake Park).
2. Density transfers are permitted to/from any residential district; however, the maximum number of residential units in the Specific Plan shall not exceed 1,860 units.
3. The land use designation of the areas transferring/receiving residential density shall be changed to the land use category matching the new density. The areas transferring/receiving residential density shall comply with the development standards of the new land use district.
4. The maximum residential density of 22 dwelling units per acre may not be exceeded without approval by the Planning Commission.
5. Density transfers that do not increase the residential unit count of the receiving site by more than 15% and/or do not result in an increase of density above 22 dwelling units per acre shall be administratively reviewed and approved by the Director of Development Services. The Director's findings may be appealed to the Planning Commission and, subsequently, to the City Council.

Phasing, Capital Improvements, and Maintenance

Infrastructure Improvements

Within Harvest Landing, the developer will be responsible for constructing the required infrastructure improvements, such as streets, water lines, sewers, storm drains, and recycled water lines.

All necessary infrastructure improvements will be developed in conjunction with the roadway improvements.

Development Phasing

The proposed phasing of parcel development is described on Table 5-1, *Development Phasing*, and shown on Figure 5-1, *Phasing Plan*. Overall, the Specific Plan is expected to be developed starting in 2008 and built out in 2014.

The roadways and infrastructure necessary to serve and provide access will be developed in conjunction with the development in each phase; however, grading may not conform to the phasing plan. Grading for the entire site may occur at one time.

It should be emphasized that the phasing program described in this section is a projection based on a judgment of future planning and market factors over the next seven years. Therefore, this phasing program is not to be taken as a compulsory development sequence. Development area sequencing may change as the result of future conditions that neither the City nor the developer has knowledge of as of the date of this submittal.

However, the basic standards will not change and are required to be complied with regardless of shifts in the composition of each development phase. The developers of property within Harvest Landing will be required to comply with all grading, drainage, and road improvements as specified in the Specific Plan.



**Table 5-1
Development Phasing**

<i>Land Use</i>	<i>Acres</i>	<i>Units</i>	<i>Square Feet</i>
Phase 1:			
Residential	34.4	270	NA
Harvest Landing Sports Park ¹	16.5	NA	NA
Paseos	2.8	NA	NA
Multiple Business Use	35.4	NA	539,708.4
Commercial	7.6	NA	73,181
<i>Total Phase 1</i>	<i>96.7</i>	<i>270</i>	<i>612,889.4</i>
Phase 2:			
Residential	30	544	NA
Multiple Business Use	45.5	NA	693,693
<i>Total Phase 2</i>	<i>75.1</i>	<i>544</i>	<i>693,693</i>
Phase 3:			
Residential	105.1	1,046	NA
Harvest Lake	11.1	NA	NA
Harvest Lake Park	8.4	NA	NA
Community Recreation Center	3.1	NA	NA
Paseos	2.2	NA	NA
Detention/Drainage	4.9	NA	NA
<i>Total Phase 3</i>	<i>134.8</i>	<i>1,046</i>	<i>NA</i>

Notes:

¹ Since a fully improved 16.5-acre Sports Park would far exceed the park requirements for the units anticipated in Phase 1, the developer may improve the Sports Park incrementally in stages; however, the Sports Park shall be fully improved in Phase 2. In Phase 1, the Sports Park may be improved per final park plans at a size proportional to the City's park requirements and the number of residential building permits issued for Phase 1 (e.g. 270 Phase 1 units equals 5 acres of improved park per final park plan). The specific park improvements shall be determined administratively by City staff and the Sports Park shall be fully improved per the final park plan in Phase 2.

In terms of the backbone circulation system, it is anticipated that the following phasing pattern will be followed. All local roadways and roadways internal to a project will be developed in conjunction with each project.

Phase 1 Circulation Improvements

- Construct Indian Avenue at its ultimate full-section width as a secondary arterial (110-foot right-of-way) between Placentia Avenue and Orange Avenue.
- Construct the intersection of Indian Avenue at Street A as a cross-street stop controlled intersection with two northbound

through lanes, a southbound left turn lane, two southbound through lanes, a westbound left turn lane, and a westbound through/right lane.

- Install a traffic signal at the intersection of Indian Avenue at Placentia Avenue and construct a westbound left turn lane.
- Install a traffic signal at the intersection of Indian Avenue at Orange Avenue and construct a southbound right turn lane, a westbound right turn lane, and an eastbound left turn lane.
- Construct Barrett Avenue at its ultimate half-section width as a project local (70-foot right-of-way) between Placentia Avenue and Orange Avenue.
- Install a traffic signal at the intersection of Barrett Avenue at Placentia Avenue and construct a northbound left turn lane, a southbound left turn lane, a westbound left turn lane and an eastbound left turn lane.
- Construct the intersection of Barrett Avenue at Street A as a cross-street stop controlled intersection with a northbound through lane, a northbound left turn lane, a southbound through lane, and westbound left and right turn lanes.
- Construct the intersection of Barrett Avenue at Orange Avenue as a cross-street stop controlled intersection with a southbound left turn lane, a southbound right turn lane, two westbound through lanes, a westbound right turn lane, two eastbound through lanes, and an eastbound left turn lane.
- Construct Barrett Avenue at its ultimate full-section width as a project collector (66-foot right-of-way) between East Frontage Road and the easterly project boundary of the commercial portion of the project.
- Construct Street A at its ultimate full-section width as a project collector (66-foot right-of-way) between Indian Avenue and Barrett Avenue.
- Construct Orange Avenue west of Indian Avenue at its ultimate full-section width as a project collector (66-foot right-of-way).



- Construct Orange Avenue at its ultimate full-section width as a secondary arterial (112-foot right-of-way) from Indian Avenue to Perris Boulevard.

Phase 2 Circulation Improvements

- Construct dual northbound left turn lanes, a southbound right turn lane, and an eastbound right turn lane at the intersection of Indian Avenue at Placentia Avenue.
- Construct Indian Avenue/Avocado Avenue at its ultimate half-section width as a secondary arterial (105-foot right-of-way) between Orange Avenue and Perris Boulevard.
- At the intersection of Indian Avenue at Orange Avenue construct a northbound left turn lane, a northbound through lane, a northbound right turn lane, a westbound left turn lane and an eastbound right turn lane.
- Construct Driveway 1 west of Avocado Avenue at its ultimate full-section width as a project collector (66-foot right-of-way).
- Install a traffic signal at the intersection of Avocado Avenue at Driveway 1 and construct a northbound left turn lane, a northbound through lane, a southbound through lane, an eastbound left turn lane and an eastbound right turn lane.
- Construct Driveway 2 south of Avocado Avenue at its ultimate full-section width as a collector project road (66-foot right-of-way).
- Install a traffic signal at the intersection of Avocado Avenue at Driveway 2 and construct a northbound left turn lane, a northbound through lane, a southbound through lane, an eastbound left turn lane and an eastbound right turn lane.
- Construct Barrett Avenue at its ultimate half-section width as a project collector (66-foot right-of-way) between Avocado Avenue and the easterly project boundary of the commercial portion of the project.
- Construct the intersection of Barrett Avenue and Avocado Avenue as a cross-street stop with a northbound left turn lane, a northbound right lane, an eastbound through lane, a westbound through lane, and a westbound left turn lane.

- Install a traffic signal at the intersection of Perris Boulevard at Avocado Avenue and construct the west leg of the intersection including right turn overlap phasing on the eastbound right turn movement, a northbound left turn lane, a third southbound through lane and a southbound right turn lane.
- At the intersection of Perris Blvd. at Orange Avenue construct a northbound left turn lane.

Phase 3 Circulation Improvements

- Install a traffic signal at Indian Avenue at Street A.
- Install a traffic signal at Barrett Avenue at Street A.
- Construct a second southbound through lane, a second northbound through lane, and implement right-turn overlap phasing on the westbound right turn movement at the intersection of Indian Avenue and Orange Avenue.
- Construct Barrett Avenue south of Orange Avenue at its ultimate full-section width as a project local (74-foot right-of-way).
- Install a traffic signal at Barrett Avenue at Orange Avenue and construct a northbound left turn lane, a northbound through lane, a southbound through lane, and a westbound left turn lane.
- Construct Driveway 1 east of Avocado Avenue at its ultimate full-section width as a project local (74-foot right-of-way).
- Construct a second northbound through lane, a second southbound through lane, a southbound left turn lane, a westbound left turn lane and a westbound through lane at the intersection of Avocado Avenue and Driveway 1.
- Construct a second northbound through lane and a second southbound through lane at the intersection of Avocado Avenue and Driveway 2.
- Install a traffic signal at the intersection of Barrett Avenue at Avocado Avenue and construct a southbound left turn lane, a southbound through lane, a second eastbound through

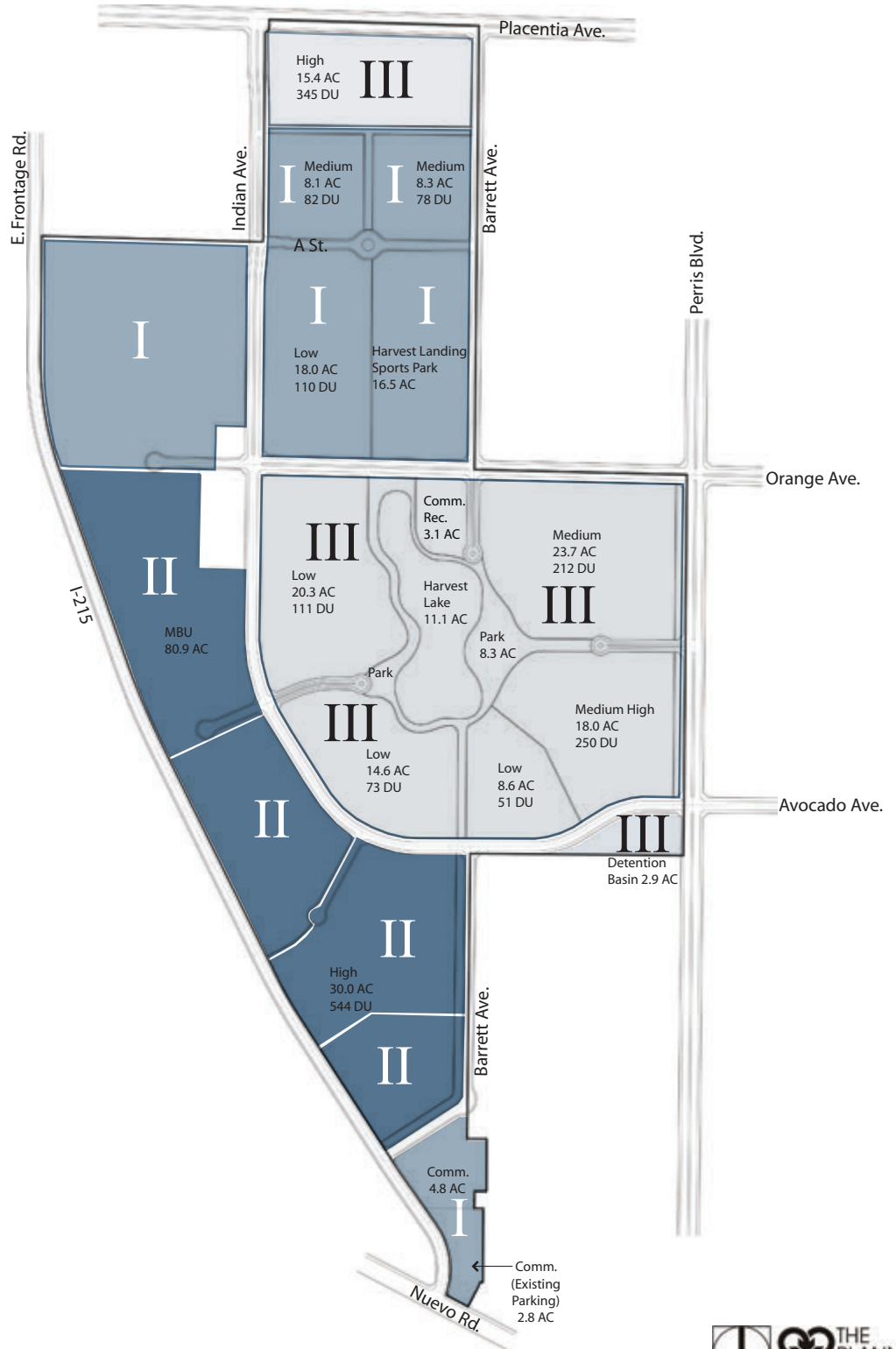


lane, an eastbound left turn lane, and a second westbound through lane.

- Perris Boulevard should be built to its ultimate half-section width (128-foot right-of-way) as a primary arterial from Orange Avenue to the southerly project boundary. This improvement consists of constructing a third southbound through lane along Perris Boulevard adjacent to the easterly project boundary.
- Construct Driveway 3 west of Perris Boulevard at its ultimate full-section width as a project local (74-foot right-of-way).
- Construct the intersection of Perris Boulevard at Driveway 3 as a cross-street stop controlled intersection with a 3rd southbound through lane, a southbound right turn lane and an eastbound right turn lane.

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PHASING PLAN



- Key:**
- Phase 1
 - Phase 2
 - Phase 3



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Maintenance

The creation and operation of a maintenance assessment district will be an important factor in maintaining the aesthetic quality of Harvest Landing. It is anticipated that maintenance responsibilities may be divided between a Master Homeowners Association, Neighborhood Associations, Community Facilities Districts (CFD), Assessment District, and/or other maintenance mechanisms. The public and private entities are described below and in Table 5-2, *Maintenance Plan*.

Master Homeowners Associations

Common areas identified within the Specific Plan shall be owned and maintained by a permanent private maintenance organization. Common areas to be owned and maintained by the Master Homeowners Association may include common recreation areas, open space, circulation systems, landscaped easements, landscaped areas at entryways and roadways, paseos, the Community Recreation Center, and the lake.

Neighborhood Homeowners Associations

In certain residential areas of the project, smaller Homeowners Associations may be created to provide maintenance responsibility for common areas and facilities that benefit only residents who reside in the immediate area.

Open Space and Parks

Open space and parks that are not directly associated with a particular neighborhood or are dedicated to the City, such as the Harvest Landing Sports Park, shall be the responsibility of the City.

Project Roadways

All public roadways shall be incorporated into the City's system of roads for operation and maintenance approved by the City. All private roads shall be owned and maintained by either the Master Homeowners Association or a Neighborhood Association.

**Table 5-2
Maintenance Plan**

<i>Type</i>	<i>Developed by</i>	<i>Owned by</i>	<i>Maintained by</i>
Streetscape			
Community Roadways (curb-to-curb: arterials, secondary arterials, and collectors)	Master Developer	City of Perris	City of Perris
Neighborhood Roadways (curb-to-curb: local streets)	Master Developer	City of Perris	City of Perris
Private Streets	Guest Builder	HOA	HOA
Community Roadway Medians (arterials, secondary arterials, and collectors)	Master Developer	City of Perris	LLD/HOA/ City of Perris
Neighborhood Roadway Medians (local streets)	Guest Builder	City of Perris	LLD/HOA/ City of Perris
Alleys	Guest Builder	HOA	HOA
Community and Secondary Entries	Master Developer	HOA	HOA
Street Lighting (community and neighborhood roadways)	Master Developer	City of Perris	LLD/HOA/ City of Perris
Community Walls and Fences	Master Developer	HOA	HOA
Interior Neighborhood Walls and Fences	Guest Builder	Homeowners	Homeowners
Parks			
Harvest Landing Sports Park	Master Developer	City of Perris	City of Perris
Harvest Lake	Master Developer	HOA	HOA
Harvest Lake Park	Master Developer	HOA	HOA
Community Recreation Center	Master Developer	HOA	HOA
Paseos			
Paseos (curb to ROW edge)	Master Developer	HOA	HOA
Infrastructure			
Water systems (on- and off-site)	Master Developer	City of Perris/EMWD	City of Perris/EMWD
Non-potable water systems	Master Developer	City of Perris	City of Perris
Sewer systems (on- and off-site)	Master Developer	City of Perris/EMWD	City of Perris/EMWD
Drainage systems (on- and off-site)	Master Developer	City of Perris/RCFC	City of Perris/RCFC

Notes:

Certain facilities and improvements may be subject to reimbursement agreements.

LLD = Landscape and Lighting District or special maintenance district

HOA = Homeowners Association (Master or Neighborhood)

RCFC = Riverside County Flood Control

EMWD = Eastern Municipal Water District



Financing Strategies

The financing of construction, operation, and maintenance of public improvements and facilities will include funding through a combination of financing mechanisms.

Various financing strategies may be utilized to fund the public facility improvements specified by the Specific Plan. Financing may involve a combination of impact fees and exactions, special assessment districts, landscaping and lighting districts, and community facilities districts, and other mechanisms as agreed to by the developer and City.

The City and developer will cooperate to ensure that the public facilities are built in accordance with all requirements of the Specific Plan. A Development Agreement and conditions of approval may be used to facilitate this process.

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APPENDICES



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APPENDIX A: LANDSCAPE PLANT PALETTE

The Landscape Plant Palette presents permitted species and minimum plant or box sizes for landscaping in different locations within the Specific Plan area. Additions to the Landscape Plant Palette are allowed with approval from the Director of Development Services as long as they comply with the guidelines in *Landscape Plants for Western Regions*, by Bob Perry (1992).

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BOTANICAL NAME	COMMON NAME	SIZE	NOTES
ENTRIES			
TREES			
<i>Erythrina bidwillii</i>	Coral Tree	24"	Accent Tree - Avg. 20' o.c. informal
<i>Jacaranda mimosifolia</i>	Jacaranda	24"	Accent Tree- Avg. 25' o.c. informal
<i>Grevillia robusta</i>	Silk Oak	24"	Accent Tree- Avg. 25' o.c. informal
<i>Pinus elderica</i>	Afghan Pine	36"	Screen Tree- Avg. 20' o.c. informal
<i>Pinus pinea</i>	Italian Stone Pine	36"	Screen Tree- Avg. 20' o.c. informal
<i>Platanus racemosa</i>	California Sycamore	36"	Accent Tree- Avg. 25' o.c. informal
<i>Tristania conferta</i>	Brisbane Box	24"	Street Tree- Avg. 25' o.c. informal
SHRUBS			
<i>Cistus spp.</i>	Rockrose	5 g	Accent Shrub
<i>Elaeagnus puegens</i>	Silverberry	5 g	Ornamental Grass
<i>Mulenbergia rigens</i>	Deer Grass	1 g	Ornamental Grass
<i>Pittosporum tobira</i>	Mock Orange	5 g	Screen Shrub
<i>Rhapiolepis indica 'Clara'</i>	Pink Indian Hawthorn	5 g	Accent Shrub
<i>Rosa banksia</i>	Lady Bank's Rose	5 g	Accent Shrub
GROUNDCOVER			
<i>Festuca spp.</i>	Marathon	Sod	Turf
<i>Festuca ovina glauca</i>	Blue Fescue	1g	
<i>Eschscholzia californica</i>	California Poppy	Flats	
<i>Rosmarinus officinalis</i>	Rosemary	1g	
ROADWAYS			
TREES			
<i>Jacaranda mimosifolia</i>	Jacaranda	24"	Accent Tree- Avg. 25' o.c. informal
<i>Grevillia robusta</i>	Silk Oak	24"	Accent Tree- Avg. 25' o.c. informal
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree	24"	Street Tree- 30' o.c.
<i>Liquidamber styraciflua 'Rotundiloba'</i>	Sweet Gum	24"	Accent Tree- Avg. 30' o.c. informal
<i>Magnolia grandiflora</i>	Southern Magnolia	24"	Street Tree- 25' o.c.
<i>Pinus elderica</i>	Afghan Pine	36"	Screen Tree- Avg. 20' o.c. informal
<i>Pinus pinea</i>	Italian Stone Pine	24"	Street Tree- 25' o.c.
<i>Pistacia chinensis</i>	Chinese Pistache	24"	Street Tree- 30' o.c.
<i>Platanus acerfolius</i>	London Plane Tree	24"	Street Tree- 30' o.c.
<i>Platanus racemosa</i>	California Sycamore	36"	Accent Tree- Avg. 25' o.c. informal
<i>Quercus agrifolia</i>	Coast Live Oak	24"	Street Tree- 30'
<i>Robinia ambigua 'Idahoensis'</i>	Idaho Locust	24"	Accent Tree- Avg. 30' o.c. informal
<i>Tristania conferta</i>	Brisbane Box	24"	Street Tree- Avg. 25' o.c. informal
<i>Zelkova serrata 'Village Green'</i>	Sawleaf Zelkova	24"	Street Tree- 30' o.c.
SHRUBS			
<i>Buxus japonica</i>	Japanese Boxwood	5 g	
<i>Cistus spp.</i>	Rockrose	5 g	Accent Shrub
<i>Elaeagnus puegens</i>	Silverberry	5g	Accent Shrub
<i>Elymus tricooides</i>	Wild Rye	1 g	Ornamental Grass
<i>Lavendula spp.</i>	Lavender	1g	Accent Shrub

BOTANICAL NAME	COMMON NAME	SIZE	NOTES
<i>Mulenbergia rigens</i>	Deer Grass	1 g	Ornamental Grass
<i>Pittosporum tobira</i>	Mock Orange	5 g	Screen Shrub
<i>Pittosporum tobira 'Wheeler's Dwarf'</i>	Mock Orange		Accent Shrub
<i>Rhapiolepis indica 'Clara'</i>	Pink Indian Hawthorn	5g	Accent Shrub
<i>Rosa banksia</i>	Lady Bank's Rose	5 g	Accent Shrub
<i>Trachelospermum jasminoides</i>	Star Jasmine	1 g	

GROUND COVER

<i>Cotoneaster horizontalis</i>	Cotoneaster	1 g	
<i>Festuca spp.</i>	Marathon	Sod	Turf
<i>Eschscholzia californica</i>	California Poppy	Flats	
<i>Hedera helix</i>	English Ivy	1 g	
<i>Lonicera japonica</i>	Japanese Honeysuckle	1 g	
<i>Myoporum pacifica</i>	Myoporum	1 g	
<i>Rosmarinus officinalis</i>	Rosemary	1g	
<i>Platanus racemosa</i>	Honey Locust	24"	Street Tree- Avg. 25' o.c. informal

PARKS & OPEN SPACE

TREES

<i>Alnus Cordata</i>	Italian Alder	24"	Spacing Varies by Design and Use
<i>Calocedrus decurrens</i>	Incense Cedar	24"	
<i>Cedrus deodara</i>	Deodar Cedar	24"	
<i>Cinnamomum camphora</i>	Camphor Tree	24"	Spacing Varies by Design and Use
<i>Cupaniopsis anacardiodes</i>	Carrot Wood	24"	
<i>Fraxinus velutina</i>	Arizona Ash	24"	
<i>Grevillia robusta</i>	Silk Oak	24"	
<i>Ginkgo biloba</i>	Maidenhair Tree	24"	
<i>Jacaranda mimosifolia</i>	Jacaranda	24"	
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree	24"	
<i>Koelreuteria panniculata</i>	Goldenrain Tree	24"	
<i>Lagerstroemia indica</i>	Crape Myrtle	24"	
<i>Laurus nobilis</i>	Sweet Bay	24"	
<i>Liriodendron tulipifera</i>	Tulip Tree	24"	
<i>Liquidambar styraciflua</i>	Sweet Gum	24"	
<i>Magnolia grandiflora</i>	Southern Magnolia	24"	
<i>Melaleuca linarifolia</i>	Flaxleaf Paperbark	24"	
<i>Pinus canariensis</i>	Canary Island Pine	24"	
<i>Pinus eldarica</i>	Afghan Pine	24"	
<i>Pinus pinea</i>	Italian Stone Pine	24"	
<i>Pistacia chinensis</i>	Chinese Pistache	24"	
<i>Platanus acerfolia</i>	London Plane Tree	24"	
<i>Platanus racemosa</i>	California Sycamore	24"	
<i>Populus nigra 'Italica'</i>	Lombardy Poplar	24"	
<i>Prunus cerasifera 'Purple Pony'</i>	Cherry Plum	24"	
<i>Prunus serrulata 'Kwanzan'</i>	Cherry Plum	24"	
<i>Pyrus callieriana</i>	Ornamental Pear	24"	



BOTANICAL NAME	COMMON NAME	SIZE	NOTES
<i>Quercus agrifolia</i>	Coast Live Oak	24"	
<i>Quercus ilex</i>	Holly Oak	24"	
<i>Robinia ambigua 'Idahoensis'</i>	Idaho Locust	24"	
<i>Sophora japonica</i>	Japanese Pagoda Tree	24"	
<i>Tristania conferta</i>	Brisbane Box	24"	
<i>Tipuana tipu</i>	Tipu Tree	24"	
<i>Ulmus parvifolia 'Drake'</i>	Chinese Evergreen Elm	24"	
<i>Umbellularia californica</i>	California Bay	24"	

SHRUBS

<i>Acacia redolens</i>	Acacia		
<i>Agapanthus africanus</i>	Lily-of-the-Nile		
<i>Buddleia davidii</i>	Butterfly Bush		
<i>Carisa grandiflora</i>	Natal Plum		
<i>Carisa macrocarpa</i>	Natal Plum		
<i>Ceanothus</i>	Wild Lilac		
<i>Cistus spp.</i>	Rockrose		
<i>Cotinus coggygria</i>	Smoke Tree		
<i>Cotoneaster horizontalis</i>	Cotoneaster		
<i>Dodonaea viscosa</i>	Hop Bush		
<i>Echium fatuosum</i>	Pride of Maderia		
<i>Eriogonum giganteum</i>	St. Catherine's Lace		
<i>Elaeagnus pungens</i>	Silverberry		
<i>Elymus tricoides</i>	Wild Rye		
<i>Escallonia fradesii</i>	Escallonia		
<i>Feijoa sellowiana</i>	Pineapple Guava		
<i>Fremontodendron californicum</i>	Common Flannel Bush		
<i>Garrya elliptica</i>	Coast Silktassel		
<i>Hemerocallis spp.</i>	Day lily		
<i>Juniperus chinensis</i>	Juniper		
<i>Juniperus sabina</i>	Savin Juniper		
<i>Lavendula spp.</i>	Lavender		
<i>Leucophyllum frutescens</i>	Texas Ranger		
<i>Leonotis Leonuris</i>	Lion's Tail		
<i>Ligustrum japonicum</i>	Japanese Privet		
<i>Ligustrum ovafolium</i>	California Privet		
<i>Melaleuca nesophila</i>	Pink Melaleuca		
<i>Morea bicolor</i>	Fortnight Lily		
<i>Muhlenbergia rigens</i>	Deer Grass		
<i>Myrsine africana</i>	African Boxwood		
<i>Myrtus communis</i>	Myrtle		
<i>Nerium oleander</i>	Oleander		
<i>Pittosporum tobira</i>	Mock Orange		
<i>Pittosporum tobira 'Wheeler's Dwarf'</i>	Mock Orange		
<i>Pittosporum tobira 'Varigata'</i>	Mock Orange		

BOTANICAL NAME	COMMON NAME	SIZE	NOTES
<i>Plumbago capensis</i>	Cape Plumbago		
<i>Podocarpus macrophyllus</i>	Yew Pine		
<i>Prunus caroliniana</i>	Carolina Laurel Cherry		
<i>Pyracantha coccinea</i>	Fire Thorn		
<i>Rhaphiolepis indica spp.</i>	Pink Indian Hawthorn		
<i>Rhamnus californica</i>	Coffeeberry		
<i>Rhus ovata</i>	Sugar Bush		
<i>Salvia luecantha</i>	Mexican Bush Sage		
<i>Salvia apiana</i>	California White Sage		
<i>Trichostema lanatum</i>	Woolly Blue Curls		
<i>Xylosma compacta</i>	Xylosma		
<i>Xylosma congestum</i>	Xylosma		

GROUND COVER

<i>Artemisia schmidtiana</i>	Angel's Hair		
<i>Baccharis pilularis</i>	Dwarf Coyote Bush		
<i>Diplacus hybrids</i>	Monkey Flower		
<i>Eschscholzia californica</i>	California Poppy		
<i>Festuca ovina glauca</i>	Blue Fescue		
<i>Festuca rubra</i>	Creeping Red Fescue		
<i>Festuca spp.</i>	Marathon		
<i>Gelsemium sempervirens</i>	Carolina Jasmine		
<i>Hedera helix</i>	English Ivy		
<i>Hemerocallis spp.</i>	Daylily		
<i>Heuchera maxima</i>	Island Alum Root		
<i>Limonium perezii</i>	Perez's Sea Lavender		
<i>Liriope muscari</i>	Lily turf		
<i>Lonicera japonica</i>	Japanese Honeysuckle		
<i>Festuca spp.</i>	Marathon Turf		Turf
<i>Myoporum 'Pacifcum'</i>	Myoporum		
<i>Pandorea jasminoides</i>	Bower Vine		
<i>Parthenocissus tricuspidata</i>	Boston Ivy		
<i>Passiflora alata caerulea</i>	Passion Vine		
<i>Rosmarinus officinalis</i>	Rosemary		
<i>Sisyrinchium bellum</i>	Blue-Eyed Grass		
<i>Trachelospermum jasminoides</i>	Star Jasmine		
<i>Trachelospermum asiaticum</i>	Asiatic Jasmine		
<i>Vinca major</i>	Periwinkle		

RESIDENTIAL, MULTIPLE BUSINESS USE, & COMMERCIAL

TREES

<i>Agonis flexuosa</i>	Willow Myrtle		
<i>Alnus cordata</i>	Italian Alder		
<i>Arbutus unedo</i>	Strawberry Tree		
<i>Brachychiton acerfolius</i>	Australian Flame Tree		
<i>Brachychiton populeneus</i>	Bottle Tree	24"	Spacing Varies by Design and Use



BOTANICAL NAME	COMMON NAME	SIZE	NOTES
<i>Callistemon viminalis</i>	Weeping Bottlebrush		
<i>Calocedrus decurrens</i>	Incense Cedar		
<i>Cedrus deodara</i>	Deodar Cedar		
<i>Cinnamomum camphora</i>	Camphor Tree	24"	
<i>Cupaniopsis anacardiodes</i>	Carrot Wood	24"	
<i>Erythrina bidwillii</i>	Coral Tree		
<i>Fraxinus velutina</i>	Arizona Ash		
<i>Geijera parviflora</i>	Strawberry Guava		
<i>Ginkgo biloba</i>	Maidenhair Tree	24"	
<i>Grevillia robusta</i>	Silk Oak		
<i>Jacaranda mimosifolia</i>	Jacaranda	24"	
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree	24"	
<i>Koelreuteria paniculata</i>	Goldenrain Tree	24"	
<i>Lagerstromia indica</i>	Crape Myrtle		
<i>Laurus nobilis</i>	Sweet Bay		
<i>Liriodendron tulipifera</i>	Tulip Tree		
<i>Liquidambar styraciflua 'Rotundiloba'</i>	Sweet Gum	24"	
<i>Magnolia grandiflora</i>	Southern Magnolia	24"	
<i>Melaleuca linarifolia</i>	Flaxleaf Paperbark	24"	
<i>Pinus canariensis</i>	Canary Island Pine	24"	
<i>Pinus eldarica</i>	Afghan Pine	24"	
<i>Pinus halepensis</i>	Aleppo Pine	24"	
<i>Pinus pinea</i>	Italian Stone Pine	24"	
<i>Pistacia chinensis</i>	Chinese Pistache	24"	
<i>Pittosporum undulatum</i>	Sweet Pittosporum		
<i>Platanus acerifolia</i>	London Plane Tree	24"	
<i>Platanus racemosa</i>	California Sycamore		
<i>Podocarpus gracillior</i>	Fern Pine	24"	
<i>Populus nigra 'Italica'</i>	Lombardy Poplar		
<i>Prunus cerasifera 'Purple Pony'</i>	Cherry Plum	24"	
<i>Prunus serrulata 'Kwanzan'</i>	Cherry Plum	24"	
<i>Pyrus calleryana</i>	Ornamental Pear	24"	
<i>Quercus agrifolia</i>	Coast Live Oak		
<i>Quercus ilex</i>	Holly Oak	24"	
<i>Rhus lancea</i>	African Sumac		
<i>Robinia ambigua 'Idahoensis'</i>	Idaho Locust		
<i>Sophora japonica</i>	Japanese Pagoda Tree		
<i>Tristania conferta</i>	Brisbane Box	24"	
<i>Tipuana tipu</i>	Tipu Tree	24"	
<i>Ulmus parvifolia 'Drake'</i>	Chinese Evergreen Elm	24"	
<i>Umbellularia californica</i>	California Bay		
<i>Zelkova serrata 'Village Green'</i>	Sawleaf Zelkova	24"	
SHRUBS			
<i>Abelia grandiflora</i>	Glossy Abelia		

BOTANICAL NAME	COMMON NAME	SIZE	NOTES
<i>Acacia redolens</i>	Acacia		
<i>Agapanthus africanus</i>	Lily-of-the-Nile		
<i>Buddleia davidii</i>	Butterfly Bush		
<i>Buxus japonica</i>	Japanese Boxwood		
<i>Carisa macrocarpa</i>	Natal Plum		
<i>Carisa grandiflora</i>	Natal Plum		
<i>Ceanothus</i>	Wild Lilac		
<i>Cistus spp.</i>	Rockrose		
<i>Cotinus coggygria</i>	Smoke Tree		
<i>Cotoneaster horizontalis</i>	Cotoneaster		
<i>Callistemon citrinus</i>	Lemon Bottlebrush		
<i>Dodonaea viscosa</i>	Hop Bush		
<i>Echium fatuosum</i>	Pride of Maderia		
<i>Elaeagnus pungens</i>	Silverberry		
<i>Eriogonum giganteum</i>	St. Catherine's Lace		
<i>Escallonia fradesii</i>	Escallonia		
<i>Feijoa sellowiana</i>	Pineapple Guava		
<i>Fremontodendron californicum</i>	Common Flannel Bush		
<i>Garrya elliptica</i>	Coast Silktassel		
<i>Grewia caffra</i>	Lavendar Star Vine		
<i>Hemerocallis spp.</i>	Day lily		
<i>Ilex spp.</i>	Holly		
<i>Juniperus chinensis</i>	Juniper		
<i>Juniperus sabina</i>	Savin Juniper		
<i>Lavendula spp.</i>	Lavender		
<i>Leonotis Leonuris</i>	Lion's Tail		
<i>Leptospermum laevigatum</i>	Australian Tea Tree		
<i>Leptospermum scoparium</i>	New Zealand Tea Tree		
<i>Leucophyllum frutescens</i>	Texas Ranger		
<i>Ligustrum japonicum</i>	Japanese Privet		
<i>Ligustrum ovalifolium</i>	California Privet		
<i>Liriope muscari</i>	Big Blue Lily Turf		
<i>Melaleuca nesophila</i>	Pink Melaleuca		
<i>Morea bicolor</i>	Fortnight Lily		
<i>Muhlenbergia rigens</i>	Deer Grass		
<i>Myrtus communis</i>	Myrtle		
<i>Myrsine africana</i>	African Boxwood		
<i>Nerium oleander</i>	Oleander		
<i>Osmanthus ilicifolius</i>	Holly-Leaf Osmanthus		
<i>Phormium spp.</i>	Flax		
<i>Pittosporum tobira</i>	Mock Orange		
<i>Pittosporum tobira 'Wheeler's Dwarf'</i>	Mock Orange		
<i>Pittosporum tobira 'Variegata'</i>	Mock Orange		
<i>Plumbago capensis</i>	Cape Plumbago		



BOTANICAL NAME	COMMON NAME	SIZE	NOTES
<i>Podocarpus macrophyllus</i>	Yew Pine		
<i>Prunus caroliniana</i>	Carolina Laurel Cherry		
<i>Pyracantha coccinea</i>	Fire Thorn		
<i>Rhamnus californica</i>	Coffeeberry		
<i>Rhaphiolepis indica spp.</i>	Pink Indian Hawthorn		
<i>Rhus ovata</i>	Sugar Bush		
<i>Rosa banksia</i>	Lady Bank's Rose		
<i>Salvia leucantha</i>	Mexican Bush Sage		
<i>Salvia apiana</i>	California White Sage		
<i>Trichostema lanatum</i>	Woolly Blue Curls		
<i>Viburnum japonicum</i>	Viburnum		
<i>Viburnum tinus spp.</i>	Viburnum		
<i>Xylosma compacta</i>	Xylosma		
<i>Xylosma congestum</i>	Xylosma		
GROUNDCOVER			
<i>Agapanthus africanus</i>	Lily-of-the-Nile		
<i>Artemisia schmidtiana</i>	Angel's Hair		
<i>Baccharis pilularis</i>	Dwarf Coyote Bush		
<i>Diplacus hybrids</i>	Monkey Flower		
<i>Eschscholzia californica</i>	California Poppy		
<i>Festuca ovina glauca</i>	Blue Fescue		
<i>Festuca rubra</i>	Creeping Red Fescue		
<i>Festuca spp.</i>	Marathon		
<i>Gelsemium sempervirens</i>	Carolina Jasmine		
<i>Heuchera maxima</i>	Island Alum Root		
<i>Hedera helix</i>	English Ivy		
<i>Hemerocallis spp.</i>	Daylily		
<i>Limonium perezii</i>	Perez's Sea Lavender		
<i>Liriope muscari</i>	Lily Turf		
<i>Lonicera japonica</i>	Japanese Honeysuckle		
<i>Festuca spp.</i>	Marathon		Turf
<i>Myoporum 'Pacificum'</i>	Myoporum		
<i>Pandorea jasminoides</i>	Bower Vine		
<i>Parthenocissus tricuspidata</i>	Boston Ivy		
<i>Passiflora alata caerulea</i>	Passion Vine		
<i>Rosmarinus officinalis</i>	Rosemary		
<i>Sisyrinchium bellum</i>	Blue-Eyed Grass		
<i>Trachelospermum jasminoides</i>	Star Jasmine		
<i>Trachelospermum asiaticum</i>	Asiatic Jasmine		



APPENDIX B: GENERAL PLAN CONSISTENCY ANALYSIS

The Harvest Landing Specific Plan is consistent with and supportive of the goals and policies of the City of Perris General Plan, as amended, to reflect the comprehensive and unified master plan for the project site. A discussion of the relationship of this Specific Plan to each of the General Plan elements follows.

Land Use Element

The Perris Land Use Element contains five goals that provide the framework for land planning and decision making within the City. The major goals that directly relate to the Specific Plan are presented below.

Goal I: Quality housing in attractive neighborhoods for households at all income levels and stages of life

Goal II: New development consistent with infrastructure capacity and municipal services capabilities

Goal III: Commerce and industry to provide jobs for residents at all economic levels

Goal IV: Consistency among all planning documents

Goal V: Protection from natural or man-made disasters

Specific Plan Response

The Specific Plan accommodates a range of land uses that will provide a balance of market driven land uses. A variety of housing opportunities, employment, and recreational opportunities will complement surrounding uses and strengthen the economic base of the area. The range of residential products envisioned will serve a wide range of the residential market from families, single professionals, first-time homebuyers, those looking to move up, empty-nesters looking to downsize, and seniors. Housing types range from single-family detached to small-lot detached, townhomes, apartments, courtyard clusters, and condominiums. In all, the plan accommodates 1,860 future and 4 existing residential units.

Design guidelines and development standards within the Specific Plan address the aesthetic integration of uses within the site and with surrounding uses. The focus is to provide for architectural, landscape, streetscape, and site design enhancements to ensure quality development and a unique setting.

The Plan includes recreational facilities and infrastructure necessary to support the planned land uses.

The Harvest Landing Specific Plan accommodates slightly less than one job per residential unit. Given the wealth of existing and planned retail uses in the immediate area, Harvest Landing accommodates a unique blend of business uses that complement these existing uses and broaden the City's job base. The approximately 81-acre Multiple Business Use area along East Frontage Road and I-215 will serve as a jobs center. This designation accommodates approximately 1.2 million square feet of business park, research and development, professional office, light industrial, and retail commercial uses.

The City of Perris General Plan placed the Harvest Landing Specific Plan site within Planning Area 4, Freeway Business Park, of the General Plan and designated the project site for a combination of specific plan, business park, and community commercial land uses. However, at the time of the writing of this Specific Plan, the new General Plan land use designations were inconsistent with the zoning designations for the site. As of 2005, the project site was zoned almost entirely Light Agriculture (A1) with a small portion on the southern tip zoned Community Commercial (CC). The Harvest Landing Specific Plan rectifies the General Plan and Zoning inconsistencies and unifies the land use designations for the site.

There are no significant natural or man-made hazards on the site. However, the location, circulation plan, and mixture of uses within the Plan area are intended to minimize noise, truck traffic, and other potential environmental impacts.



Circulation Element

The Perris Circulation Element is designed to provide a safe, efficient, and adequate circulation system that will provide adequate capacity for future land uses in the City. The following goals relate to the Specific Plan:

Goal I: A comprehensive transportation system that will serve projected future travel demand, minimize congestion, achieve the shortest feasible travel times and distances, and address future growth and development in the City

Goal II: A well-planned, -designed, -constructed, and -maintained street and highway system that facilitates the movement of vehicles and provides safe and convenient access to surrounding developments

Goal IV: Safe and convenient pedestrian access and nonmotorized facilities between residential neighborhoods, parks, open space and schools that service those neighborhoods.

Goal V: Efficient goods movement

Specific Plan Response

The Specific Plan supports roadways of regional significance (e.g. Perris Boulevard and Placentia Avenue) that serve thru-traffic and project-generated traffic as well as local, internal roadways that connect and serve the land uses within the plan itself. The roadways are designed to match and/or exceed the City's roadway standards to provide an efficient capacity to accommodate on- and off-site vehicular traffic.

Pedestrian and bicycle paths connect the major features of the site and provide a safe, off-road system of travel. In addition, Harvest Landing connects to the surrounding circulation pattern.

The General Plan circulation system is amended in conjunction with this Plan to create a safe and efficient pattern of circulation. Specifically, Indian Avenue is realigned to create a 90-degree intersection with Perris Boulevard.

Conservation

The Perris Conservation Element addresses issues and opportunities to conserve the City's sensitive lands and resources. The following goals relate to the Specific Plan.

Goal I: Agricultural Resources. Orderly conversion of agricultural lands

Goal II: Biological Resources. Preservation of areas with significant biotic communities

Goal IV: Cultural Resources. Protection of historical, archaeological, and paleontological sites

Goal V: Water Supply. An adequate water supply to support existing and future land uses, anticipated in the Land Use Element

Goal VI: Water Quality. Achieve regional water quality objectives and protect the beneficial uses of the region's surface and groundwater.

Specific Plan Response

Harvest Landing represents an orderly conversion of agricultural lands. The Specific Plan responds to the City's General Plan, which designates the site for urban land uses. The General Plan places the Harvest Landing Specific Plan site within Planning Area 4, Freeway Business Park, of the General Plan and designates the project site for a combination of specific plan, business park, and community commercial land uses. The proximity of Planning Area 4 to I-215 makes it ideal for freeway-oriented land uses, business park, and freeway-oriented business.

No significant biologic or cultural resources were discovered on the site. A water supply assessment was prepared for Harvest Landing and found that there is adequate water supply to accommodate full buildout of the project.

The Plan includes built-in water detention basins to achieve the City's Storm Water Pollution Prevention Program.

The Harvest Landing Specific Plan requires that the responsibility for improvements to the infrastructure and utility systems necessitated by new development be borne by the new development benefiting from the improvements, either through the payment of fees, or the actual construction of the improvements.



Housing Element

The Perris Housing Element addresses housing needs within the City. It focuses on the creation of a mix of housing types, the provision of affordable housing, equal housing opportunities for all residents, and the preservation and improvement of existing neighborhoods. The following goals from the Housing Element relate to the Specific Plan.

Goal I: Promote and maintain a balance of housing types and corresponding affordability levels to provide for the community's demands for housing within all economic segments of the City.

Goal VI: Equal housing opportunity for all residents of Perris, regardless of race, religion, sex, marital status, ancestry, national origin, color or handicap.

Specific Plan Response

Harvest Landing includes a wide array of housing types and densities to accommodate different lifestyles and lifestages. Residential products found within Harvest Landing include single-family detached homes, small-lot detached homes, townhomes, apartments, courtyard clusters, and condominiums. These residential products range from 5 to 22 units per acre and are intended to serve a wide range of the residential market—families, single professionals, first-time homebuyers, those looking to move up, empty-nesters looking to downsize, and seniors.

The Low Density Residential land use designation provides for 345 detached single family units. The Medium Density Residential designation provides for 372 small-lot detached or attached products. The Medium High Density Residential designation provides for 250 attached units. Finally, the High Density Residential designation provides for 889 attached units. This mix of densities and product types is intended to maximize lifestyle options and provide for a wide range of home buyers.

Noise Element

The purpose of the Perris Noise Element is to develop strategies to reduce excessive noise resulting from traffic, rail, and stationary sources such as air conditioners and construction activity. The following goals relate to the Specific Plan.

Goal I: Land Use Siting. Future land uses compatible with projected noise environments

Specific Plan Response

Development within Harvest Landing will comply with the City's interior and exterior noise standards. In conjunction with plan review, the City will review noise attenuation prior to issuance of building permits.

The majority of proposed residential properties in Harvest Landing are protected from noise originating on I-215 by the Multiple Business Use area. Residential uses will also be buffered from traffic noise by a system of paseos, landscaped parkways, and a drainage swale along Perris Boulevard. In addition, residences located along I-215 will be buffered from the noise of the freeway through the utilization of double-paned windows, HVAC units, air filters, and entries oriented away from the freeway.

Harvest Landing is not located in the noise contours of the March Air Reserve Base/March Global Port and significant aircraft related noise is not an issue.

Facilities for active recreational uses that are likely to draw cheering crowds, elicit loud play, have amplified game announcements (e.g., stadiums, soccer fields, tennis courts, basketball courts, etc.), or are illuminated for nighttime play are located within the park's interior and away from surrounding residential uses. Lights and speakers are directed away from residences to minimize spillover. Lights are hooded and directed to minimize spillover and speakers are located near the ground and distributed evenly to minimize the need for high volume. Speakers are limited to daylight hours and lights are turned off by 10:00 P.M.



Open Space Element

The Perris Open Space Element addresses the provision of open space and recreational facilities in Harvest Valley. The following goals relate to the Specific Plan.

Goal I: Recreational opportunities that are available to all members of the community

Goal II: Establish comprehensive trail system for pedestrian, bicycle and equestrian use

Specific Plan Response

Harvest Landing provides a wealth of recreational opportunities, including a lake, regional sports park, and paseo system.

The 11.1-acre Harvest Lake is envisioned to be a recreational feature and is designed to accommodate nonmotorized water activities like paddle-boating, canoeing, fishing, and sailing. The lake will have a meandering trail so residents and visitors can stroll around and enjoy this amenity. In combination with the lake, a 3.1-acre private recreation center, which provides various recreation opportunities and facilities, such as sports courts, a community pool, and a clubhouse, will form the hub of activity for the community.

Harvest Landing also features the 16.5-acre Harvest Landing Sports Park, which is located north of Orange Avenue along Barrett Avenue. This park will provide for active recreation uses, such as soccer and baseball fields, sports courts, and tot lots.

A system of paseos is located throughout the site and enhances the Plan's recreational opportunities, provides an alternative and safe means of traveling through the site without the automobile, and promotes a healthy lifestyle for the residents. Paseos are generally located around the lake, along Indian and Orange Avenues, and in the interior of the site. The paseos accommodate pedestrian circulation and provide additional opportunities for active and passive recreation uses, such as jogger's exercise courses and picnic areas.

Safety Element

The Perris Safety Element identifies natural and man-made hazards and establishes policy to minimize exposure to the hazards. The following goals relate to the Specific Plan:

Goal 1: Reduced risk of damage to property or loss of life due to a natural or man-made disasters

Specific Plan Response

Harvest Landing is not located in known natural or man-made hazard areas. The Multiple Business Use area allows uses that may utilize hazardous materials. The use of any listed hazardous materials in the City will be subject to review by the Riverside County Fire Department.



APPENDIX C: MARKET ANALYSIS

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Retail Opportunities for City of Perris and Coudures Property

**Presented to :
McWalters & Kelterer
November, 2005**

Prepared by:
Gregory Stoffel & Associates

Table of Contents

- Taxable Sales Performance
- Demographic Comparison
- Significant Retailer Opportunities
- Observations and Conclusions

Taxable Sales Performance Summary

City of Perris Taxable Sales Rankings (Compared to 28 Other Selected Cities)						
<u>Retail Category</u>	<u>Total Number of Stores</u>	<u>Number of Stores Rank</u>	<u>Population Per Store Rank</u>	<u>Total Category Sales Rank</u>	<u>Average Sales Per Store Rank</u>	<u>Average Sales Per Capita Rank</u>
Apparel	44	19	16	21	21	21
General Merchandise	15	23	10	26	26	24
Food Stores	36	13	25	21	25	13
Eating & Drinking	67	23	20	24	20	21
Home Furnishings & Appliances	11	27	1	26	21	25
Building Materials	11	21	11	21	18	21
Auto Dealers & Supplies	29	23	8	23	21	22
Service Stations	12	25	16	12	1	2
Other/Specialty Retail Stores	143	25	5	18	8	14
Retail Stores Total **	368	24	10	25	21	23
All Other Outlets **	346	24	6	10	2	7
Total - All Outlets **	714	23	8	23	12	20
GAFO Categories	213	23	8	26	21	23
Population Rank	21	21	21	21	21	21

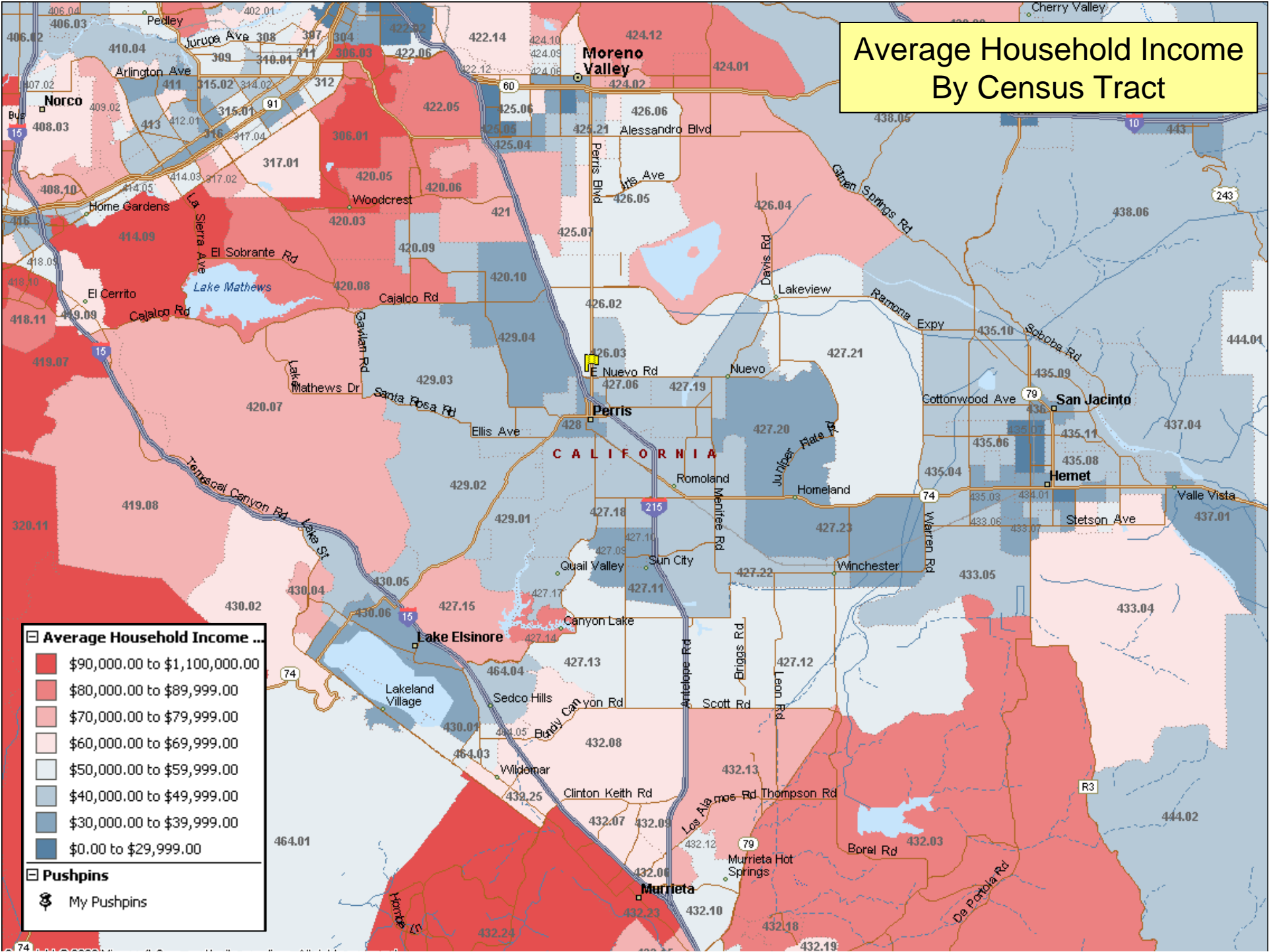
Note: Red numbers indicate rankings in upper half of compared cities.

** Note: These three categories compare 31 cities, as compared to 27 cities in other categories.

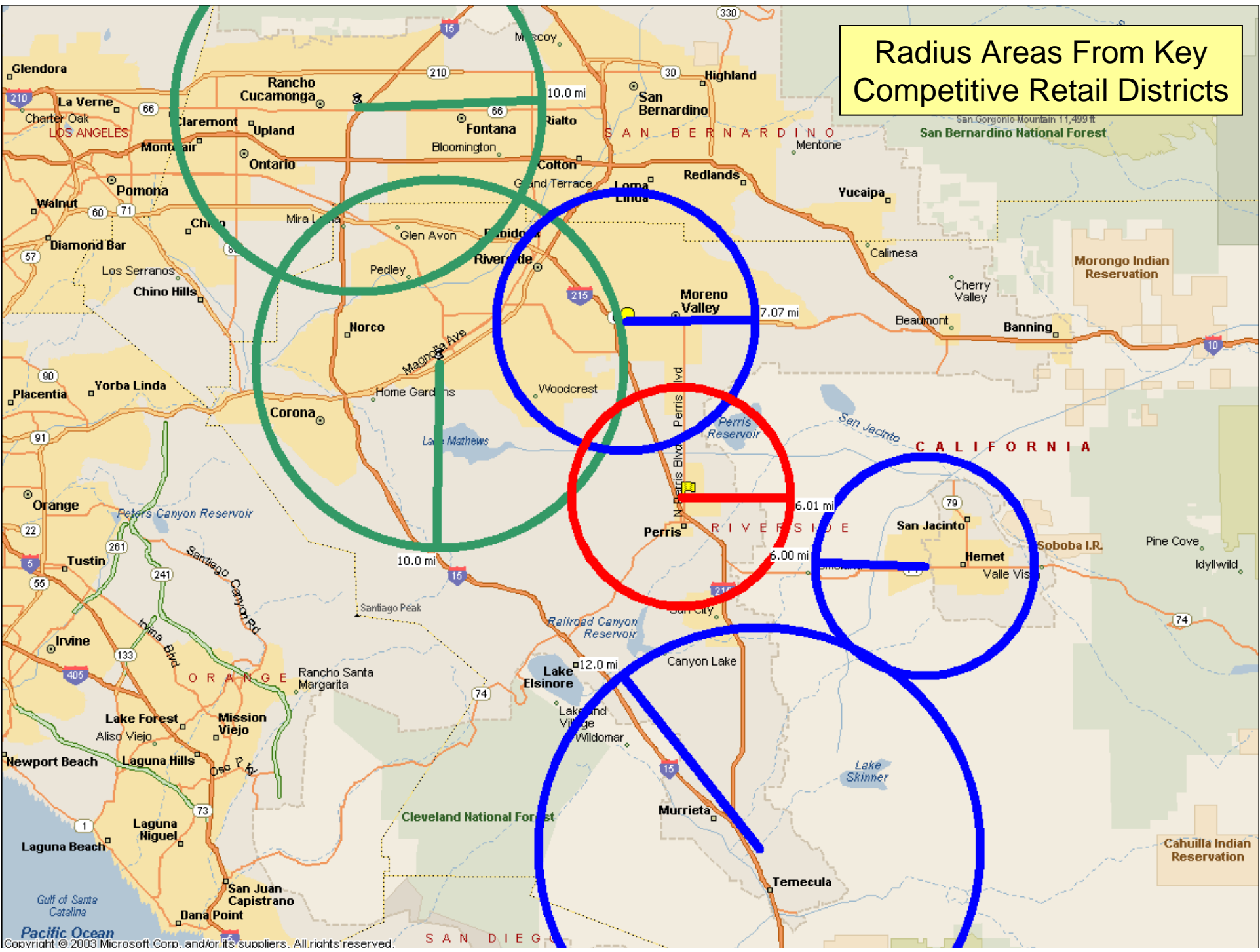
Four cities have insufficient category sales for the State Board to release due to confidentiality.

Source: State Board of Equalization, Gregory Stoffel & Associates.

Average Household Income By Census Tract



Radius Areas From Key Competitive Retail Districts



Demographic Comparison of Shopping Districts

Based Upon Four-Mile Radius (Arranged by Average Per Capita Income)				
	Perris Site 4-Mile	Hemet Mall 4-Mile	Canyon Springs 4-Mile	Temecula Mall 4-Mile
Population - 2009 Projection	54,741	112,536	171,588	121,279
Population - 2004	49,405	100,592	152,253	94,022
Households - 2009 Projection	14,059	43,296	50,527	38,245
Households - 2004	13,002	39,319	45,265	29,969
2005 Avg. Household Size	3.77	2.5	3.3	3.1
<u>Ethnicity:</u>				
White Alone	37.68%	74.82%	48.53%	75.99%
Black or African American Alone	1.61%	3.00%	16.58%	3.94%
American Indian and Alaska Native Alone	1.49%	1.55%	0.85%	0.82%
Asian Alone	1.49%	1.45%	8.37%	5.12%
Native Hawaiian and Other Pacific Islander Alc	2.33%	0.15%	0.49%	0.33%
Some Other Race Alone	0.32%	14.72%	19.45%	9.22%
Two or More Races	37.53%	4.30%	5.74%	4.59%
Not Hispanic or Latino	38.5%	68.5%	62.6%	78.5%
Hispanic or Latino	61.5%	31.5%	37.4%	21.5%
Speak Only English at Home	51.7%	76.8%	67.6%	80.7%
Est. Median Age - 2004	26.70	38.84	28.13	31.30
Est. Average Age - 2004	29.81	41.55	30.94	32.63

Demographic Comparison of Shopping Districts

	Perris Site 4-Mile	Hemet Mall 4-Mile	Canyon Springs 4-Mile	Temecula Mall 4-Mile
<u>Educational Attainment - Pop. 25+:</u>	26,001	66,086	82,868	54,874
Less than 9th grade	18.5%	9.8%	8.4%	4.0%
Some High School, no diploma	21.0%	17.9%	12.1%	8.3%
High School Graduate (or GED)	26.3%	29.9%	21.9%	23.3%
Some College, no degree	22.4%	26.4%	26.8%	34.2%
Associate Degree	5.5%	5.6%	8.9%	8.3%
Bachelor's Degree	4.5%	6.6%	12.9%	15.4%
Master's Degree	1.1%	2.8%	5.9%	4.7%
Professional School Degree	0.5%	0.9%	1.7%	1.4%
Doctorate Degree	0.1%	0.2%	1.5%	0.5%
Cume: Bachelor's Degree or Higher	6.2%	10.5%	22.0%	22.0%
Number With Bachelor's Degree or Higher	1,617	6,913	18,206	12,078
<u>2005 Household Income:</u>	13,002	39,319	45,265	29,969
Income Less than \$15,000	17.9%	21.9%	11.9%	8.4%
Income \$15,000 - \$24,999	14.8%	19.3%	9.9%	7.2%
Income \$25,000 - \$34,999	15.5%	15.5%	10.8%	9.2%
Income \$35,000 - \$49,999	18.8%	16.8%	14.7%	13.1%
Income \$50,000 - \$74,999	18.4%	14.6%	20.2%	22.7%
Income \$75,000 - \$99,999	8.7%	6.6%	14.8%	16.4%
Income \$100,000 - \$149,999	5.0%	3.9%	12.7%	15.6%
Income \$150,000 - \$249,999	0.7%	1.0%	4.1%	5.7%
Income \$250,000 - \$499,999	0.2%	0.3%	0.8%	1.3%
Income \$500,000 and more	0.0%	0.1%	0.2%	0.3%
Households \$75,000 and above	1,903	4,683	14,725	11,799
Households \$100,000 and above	772	2,076	8,044	6,875
Average Household Income	\$44,090	\$40,202	\$64,936	\$75,164
Median Household Income	\$36,431	\$30,681	\$53,425	\$63,320
Per Capita Income	\$11,845	\$16,064	\$19,476	\$24,113

Demographic Comparison of Shopping Districts

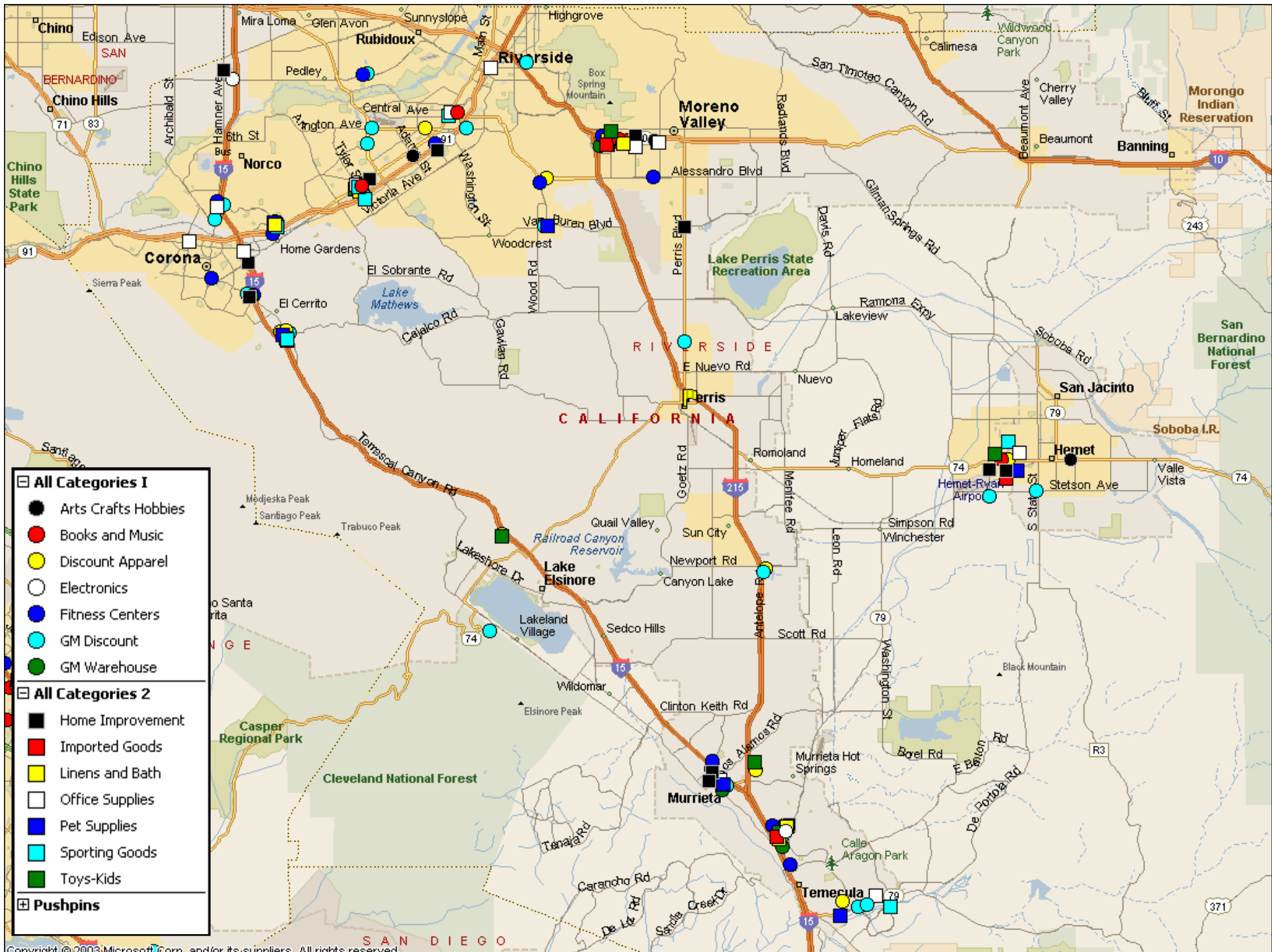
	Perris Site 4-Mile	Hemet Mall 4-Mile	Canyon Springs 4-Mile	Temecula Mall 4-Mile
Average Household Income	\$44,090	\$40,202	\$64,936	\$75,164
Median Household Income	\$36,431	\$30,681	\$53,425	\$63,320
Per Capita Income	\$11,845	\$16,064	\$19,476	\$24,113
Aggregate Total Income (Millions)	\$585.2	\$1,615.9	\$2,965.3	\$2,267.2
<u>Population 16+ By Occupation:</u>	16,541	31,384	66,021	41,451
Management, Business, and Financial Operations	7.4%	8.0%	11.8%	14.1%
Professional and Related Occupations	10.7%	14.5%	21.3%	18.6%
Service	17.6%	22.0%	14.0%	15.2%
Sales and Office	24.4%	24.8%	29.3%	29.9%
Farming, Fishing, and Forestry	0.5%	1.6%	0.3%	0.3%
Construction, Extraction and Maintenance	15.4%	12.8%	9.7%	9.9%
Production, Transportation and Mat'l. Moving	24.1%	16.3%	13.6%	12.0%
Blue Collar	39.5%	29.1%	23.3%	21.9%
White Collar	42.3%	47.0%	62.3%	62.3%
Service and Farm	18.2%	23.9%	14.4%	15.8%
Owner Occupied Housing Units	69.1%	65.0%	64.7%	71.5%
Renter Occupied Housing Units	30.9%	35.0%	35.3%	28.5%
Source: Claritas Inc., 2005.				
Compiled by: Gregory Stoffel & Associates.				

Retail Tenant Opportunities

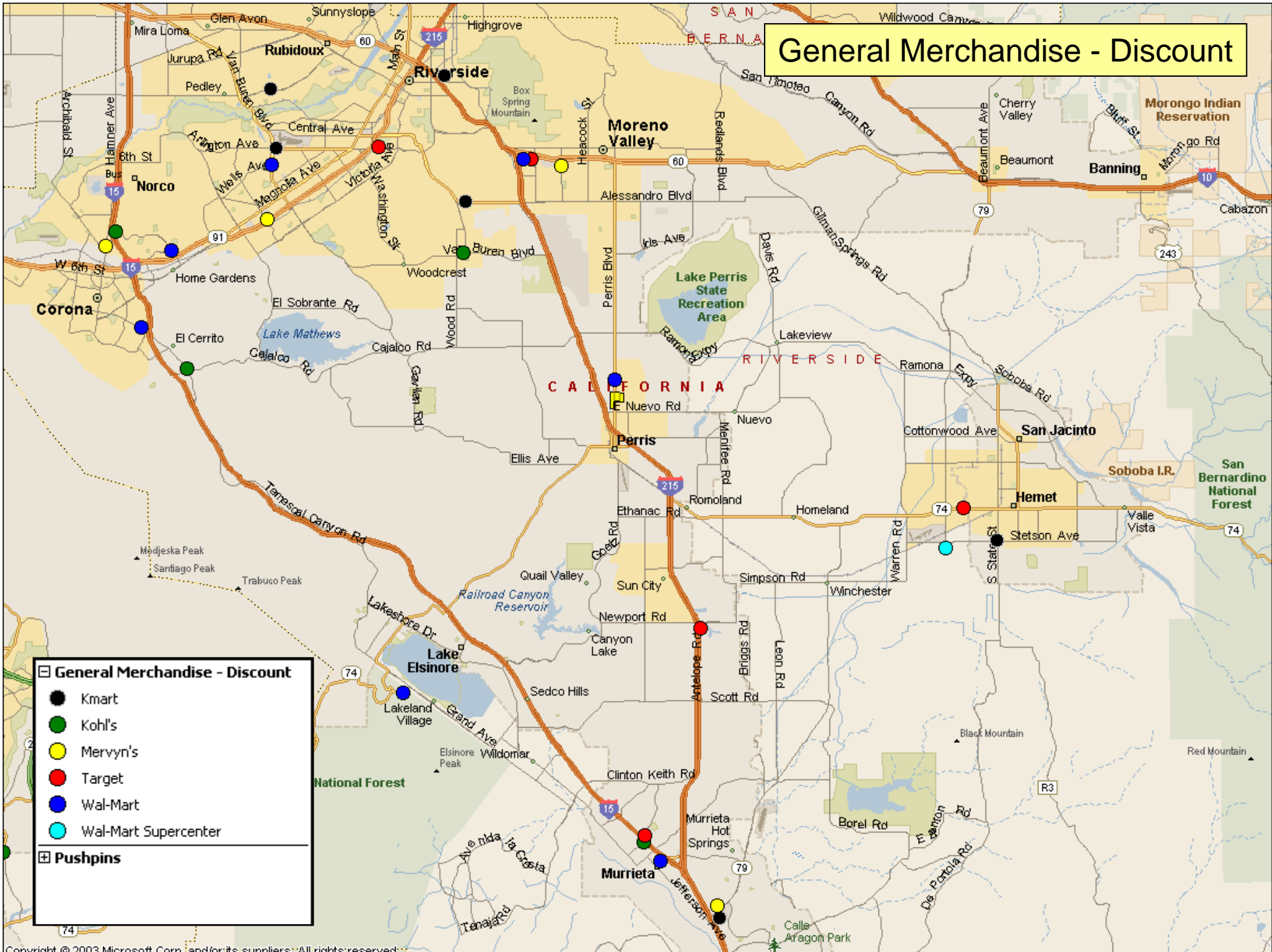
- The following section represents a tactical approach to understanding the retail development potential for the greater Perris market area.
- In order to understand the opportunity for retail development in the greater Perris market area, it is important to understand how retailers view the greater 215 Corridor market area and how they locate relative to that point of view.
- It is also important to understand how these locations influence regional patronage patterns.
- Retail tenants already located in easily accessible portions of the market area are likely not candidates for a new site unless their current locations limit expansion potential.
- Retail tenants not located or under-represented in the market area may represent potential opportunities.
- Significant tenants are typically non-mall value or discount tenants who tend to locate in larger regional destination centers and specialty-oriented centers and districts.

Retail Tenant Opportunities

- Significant retailer categories examined include:
 - Anchors – General Merchandise, Discount
 - Anchors – General Merchandise, Warehouse
 - Anchors – Home Improvement
 - Apparel - Discount
 - Arts, Crafts and Hobbies
 - Books and Music
 - Electronics
 - Fitness Centers
 - Grocery/Specialty
 - Imported Goods
 - Linens and Bath
 - Movie Theaters (Not mapped)
 - Office Supplies
 - Pet Supplies
 - Sporting Goods
 - Toys/Kids
- For purposes of this presentation, retail tenant opportunities are summarized.

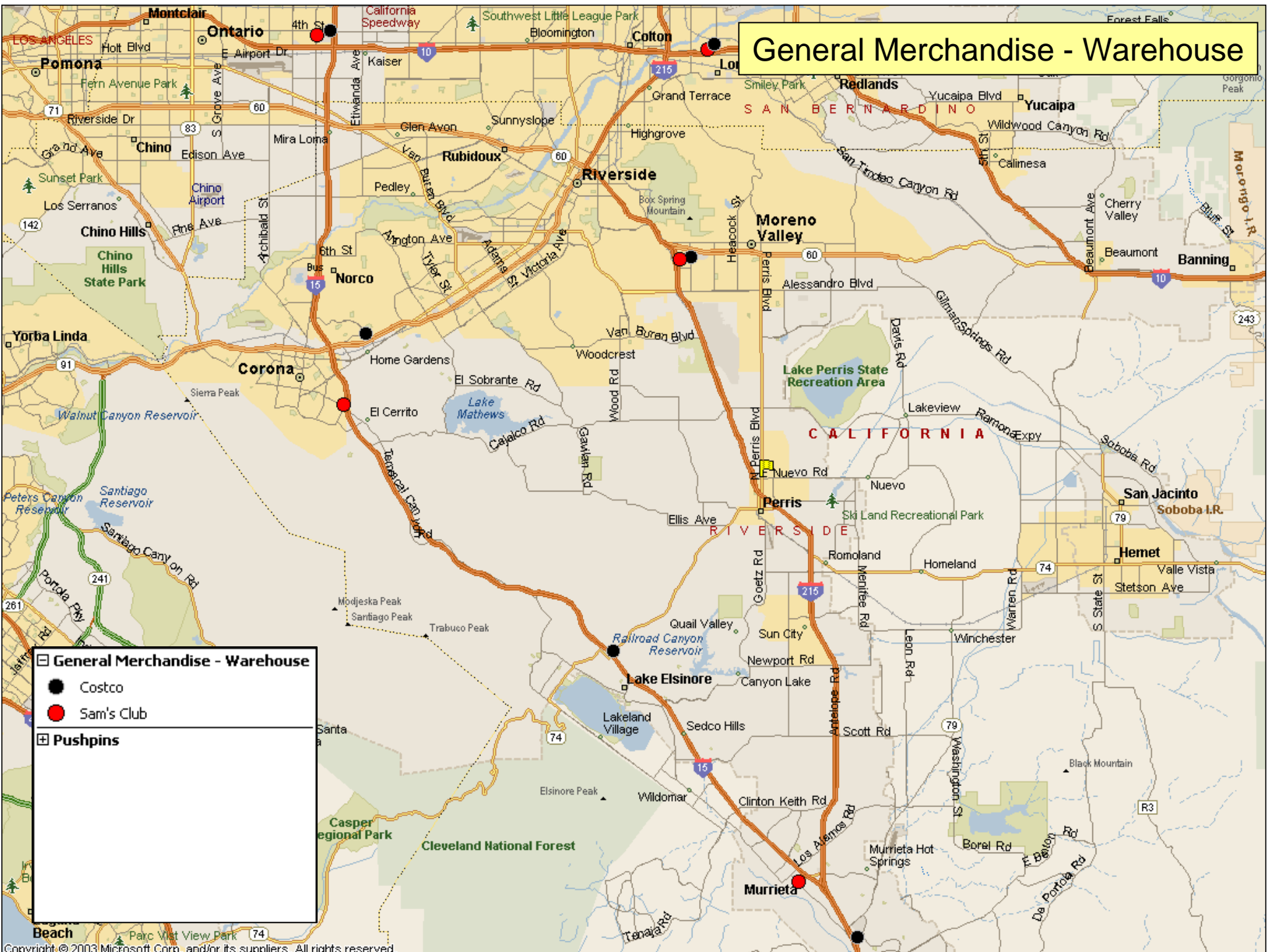


General Merchandise - Discount



	General Merchandise - Discount
	Kmart
	Kohl's
	Mervyn's
	Target
	Wal-Mart
	Wal-Mart Supercenter
	Pushpins

General Merchandise - Warehouse



- General Merchandise - Warehouse
- Costco
- Sam's Club
- Pushpins

Significant Retailer Opportunities

SIGNIFICANT TENANT LOCATIONS IN CITY OF PERRIS MARKET AREA				Approx.
(Not including relocations)				Sq. Ft.
			<u>Comments</u>	
<u>Anchor - GM Discount</u>				
Target	Nearest Also in Also in	Menifee Moreno Valley Hemet	Opportunity with population growth	135
Wal-Mart	Nearest Also in Also in	Perris Moreno Valley Murrieta	New Supercenter opening in 2006	200
Kohl's	Nearest Also in Also in	Riverside - Van Buren Murrieta Temecula	Next location would be Moreno Valley	
Mervyn's	Nearest Also in Also in	Moreno Valley Temecula Riverside - Tyler	Not doing locations like Perris	
Kmart / Sears Grand	Nearest Also in Also in	Riverside - Alessandro Hemet Temecula	May be too close to Hemet	
<u>Anchor - GM Warehouse</u>				
Costco	Nearest Also in	Moreno Valley Temecula	Population too low for spacing	
Sam's Club	Nearest Also in	Moreno Valley Murrieta	Population too low for spacing	

Grocery Stores



Grocery Chain Stores	
●	Albertsons
●	Food 4 Less
●	Stater Bros Markets
●	Wal-Mart Supercenter
Pushpins	
●	Smaller Markets

Significant Retailer Opportunities

SIGNIFICANT TENANT LOCATIONS IN CITY OF PERRIS MARKET AREA

(Not including relocations)

			<u>Comments</u>	<u>Approx. Sq. Ft.</u>
<u>Anchor - Home Improvement</u>				
Home Depot	Nearest Also in Also in Also in	Moreno Valley (6 mi.) Moreno Valley (10 mi.) Lake Elsinore Hemet	Opportunity with population growth	
Lowe's H/I	Nearest Also in Also in Also in	Moreno Valley Lake Elsinore Hemet Murrieta	Opportunity with population growth	135
OSH	Nearest	Rancho Cucamonga	-----	
<u>Anchor - Grocery Specialty</u>				
Trader Joe's	Nearest Also in	Riverside Temecula	Incomes are too low	
Whole Foods	Nearest	No store in region.	Incomes are too low	
Bristol Farms	Nearest	No store in region.	Incomes are too low	
99 Ranch Market	Nearest	No store in region.	Asian population too low	

Apparel - Discount



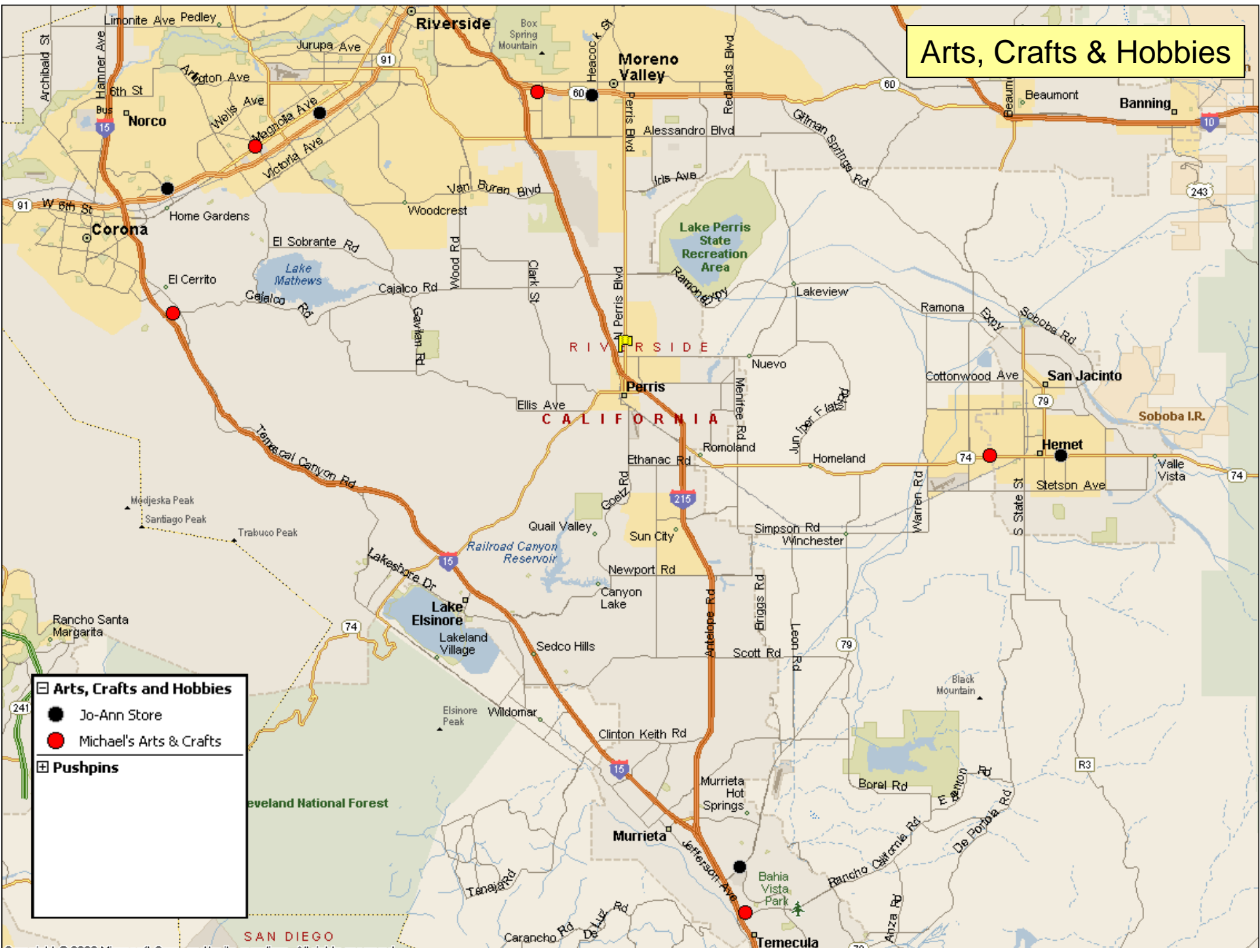
Significant Retailer Opportunities





SIGNIFICANT TENANT LOCATIONS IN CITY OF PERRIS MARKET AREA

(Not including relocations)

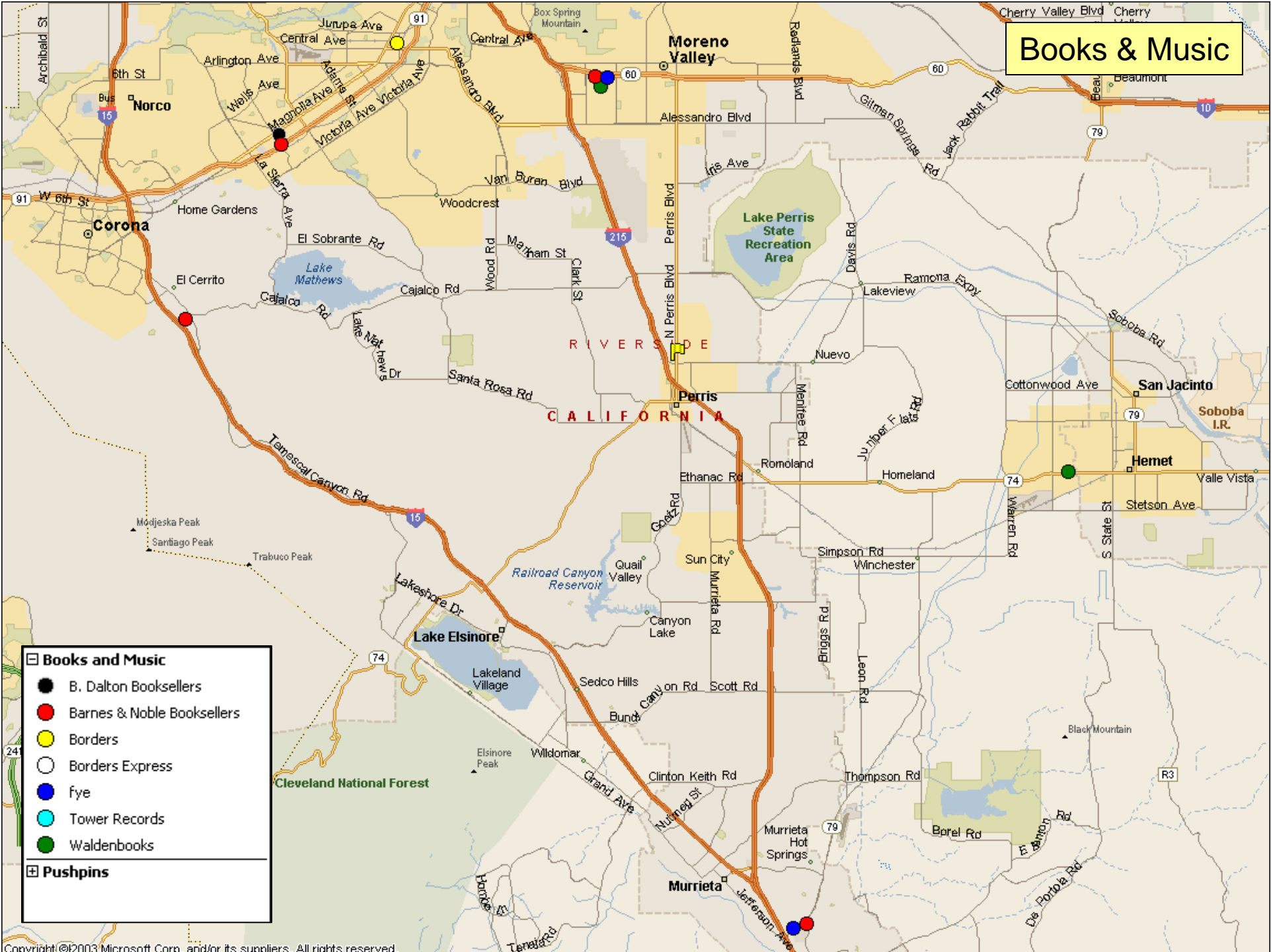
			<u>Comments</u>	<u>Approx. Sq. Ft.</u>
<u>Apparel - Discount</u>				
Old Navy	Nearest Also in	Lake Elsinore Temecula	Next location would be Moreno Valley	
Ross	Nearest Also in	Moreno Valley Menifee	Opportunity with population growth	30
TJ Maxx	Nearest Also in	Temecula Ontario	Incomes may be too low.	
Marshall's	Nearest Also in	Temecula Corona	Opportunity with population growth	30
SteinMart	Nearest Also in	Riverside - Alessandro Temecula	Incomes are too low	
Tilly's	Nearest Also in	Temecula Corona	Not a good market for concept	
Burlington	Nearest Also in	Riverside - Tyler Ontario	Population too low	
Nordstrom Rack	Nearest Also in	Ontario Chino	Population and incomes too low	

Arts, Crafts & Hobbies



-  Arts, Crafts and Hobbies
-  Jo-Ann Store
-  Michael's Arts & Crafts
-  Pushpins

Books & Music



Electronics



Electronics
Best Buy
Circuit City
CompUSA
Fry's Electronics
Pushpins

Significant Retailer Opportunities

SIGNIFICANT TENANT LOCATIONS IN CITY OF PERRIS MARKET AREA

(Not including relocations)

			<u>Comments</u>	<u>Approx. Sq. Ft.</u>
<u>Arts & Crafts</u>				
Michael's	Nearest Also in Also in	Moreno Valley Hemet Temecula	Population too low for spacing	
JoAnn Fabrics	Nearest Also in Also in	Moreno Valley Hemet Temecula	Population too low for spacing	
<u>Books/Music</u>				
Borders/B. Express/Walden	Nearest Also in Also in Also in	Riverside - Riv. Plaza Temecula Moreno Valley (Walden) Hemet (Waldenbooks)	Education levels too low	
Barnes & Noble/B. Dalton	Nearest Also in Also in	Moreno Valley Temecula Riverside - Tyler	Education levels too low	
FYE	Nearest Also in	Moreno Valley Temecula	Too close to Moreno Valley	
<u>Electronics</u>				
Best Buy	Nearest Also in Also in	Moreno Valley Murrieta Corona	Spacing not good	
Circuit City	Nearest Also in Also in	Moreno Valley Murrieta Corona	Spacing not good	
CompUSA (Good Guys)	Nearest Also in	Rancho Cucamonga Mission Viejo	Incomes are too low	
Fry's	Nearest	Anaheim	Population too low	



Linens and Bath



- Linens and Bath**
- Anna's Linens
- Bed Bath and Beyond
- Linens 'n Things
- Pushpins**

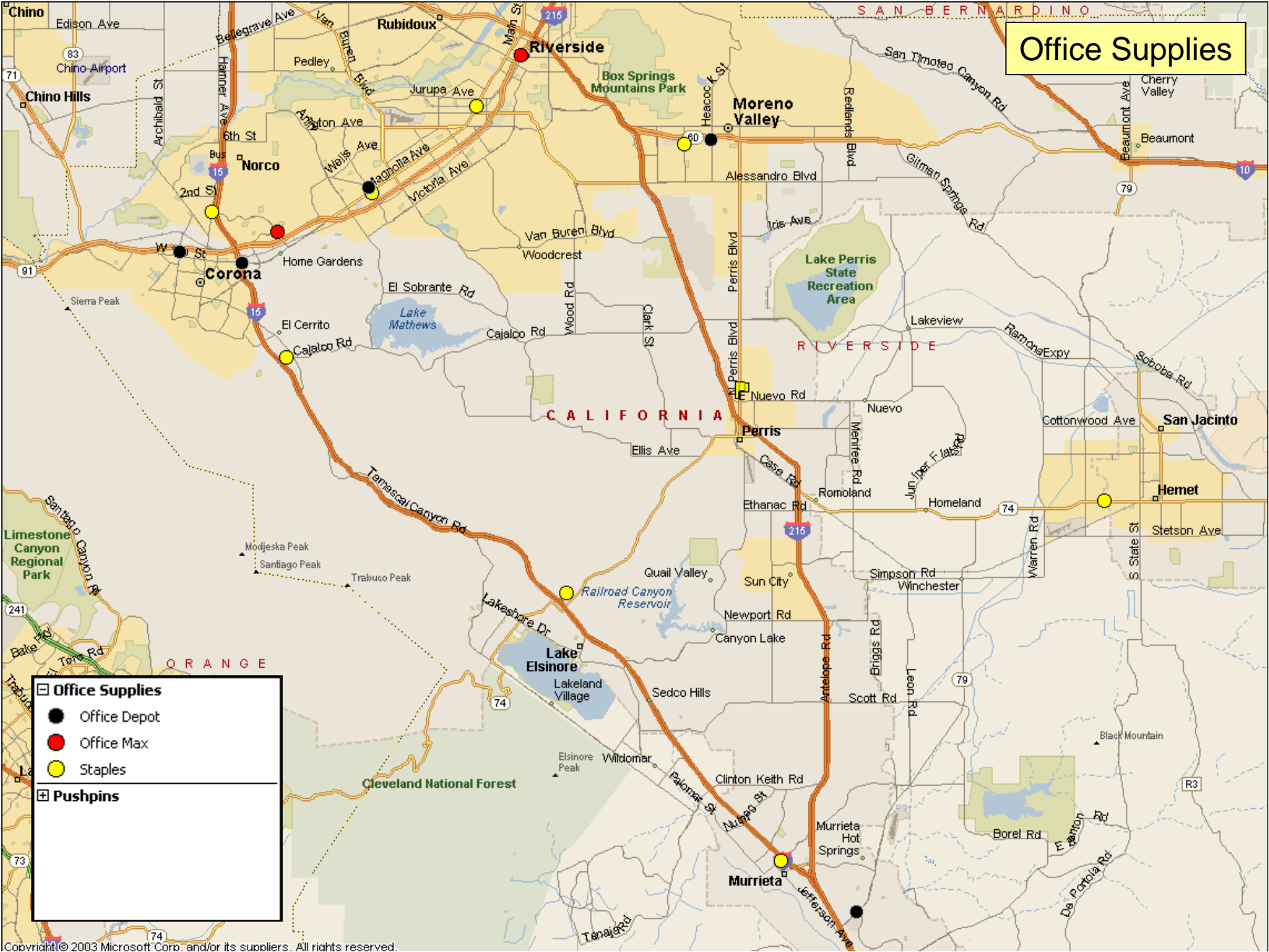
Significant Retailer Opportunities

SIGNIFICANT TENANT LOCATIONS IN CITY OF PERRIS MARKET AREA

(Not including relocations)

			<u>Comments</u>	<u>Approx. Sq. Ft.</u>
<u>Fitness Centers</u>				
24-Hour Fitness	Nearest Also in Also in	Moreno Valley Temecula Corona (2)	Opportunity with population growth	25
LA Fitness	Nearest Also in Also in	Riverside - Canyon Springs Riverside - Mission Village Murrieta	Opportunity with population growth	
Gold's Gym	Nearest	Temecula	Opportunity with population growth	
Bally's	Nearest	Riverside - Madison	Not very active in market	
<u>Imported Goods</u>				
Pier 1	Nearest Also in Also in	Riverside - Canyon Springs Hemet Temecula	Spacing not good	
Cost Plus	Nearest Also in Also in	Temecula Corona Redlands	Incomes are too low	
<u>Linens & Bath</u>				
Anna's Linens	Nearest Also in	Moreno Valley Temecula	Spacing not good	
Bed Bath Beyond	Nearest Also in	Riverside - Tyler Corona	Incomes are too low	
Linens n Things	Nearest Also in	Riverside - Canyon Springs Temecula	Incomes are too low	

Office Supplies



	Office Supplies
	Office Depot
	Office Max
	Staples
	Pushpins

Significant Retailer Opportunities

SIGNIFICANT TENANT LOCATIONS IN CITY OF PERRIS MARKET AREA

(Not including relocations)

			<u>Comments</u>	<u>Approx. Sq. Ft.</u>
<u>Office Supplies</u>				
Office Depot	Nearest Also in	Moreno Valley Temecula	Opportunity with population growth	25
Office Max	Nearest Also in	Riverside - Vine St. Corona	Not doing many deals in low income areas	
Staples	Nearest Also in Also in Also in	Moreno Valley Lake Elsinore Hemet Murrieta	Spacing not good	
<u>Pets</u>				
Petsmart	Nearest Also in Also in Also in	Riverside - Canyon Springs Lake Elsinore Murrieta Temecula	Spacing not good	
Petco	Nearest Also in Also in	Riverside - Off Van Buren Hemet Temecula	Spacing not good	
<u>Anchor - Movie Theaters</u>				
Cinema Star Ultrplex 10 Regal Hemet 12 Signature Metro 12 Movie Experience 16 Cinema Star Mission Grove 18		Perris Hemet San Jacinto Murrieta Riverside	Movies are over-supplied	

Sporting Goods



Sporting Goods

- Big 5
- Chick's Sporting Goods
- Sport Chalet
- Sportmart

Pushpins

Significant Retailer Opportunities

SIGNIFICANT TENANT LOCATIONS IN CITY OF PERRIS MARKET AREA

(Not including relocations)

			<u>Comments</u>	<u>Approx. Sq. Ft.</u>
<u>Sporting Goods</u>				
Big 5	Nearest Also in Also in	Moreno Valley Hemet Murrieta	Opportunity with population growth	
Sportmart	Nearest Also in Also in	Temecula Riverside - Tyler Corona	Next location would be Moreno Valley	
Sport Chalet	Nearest	Temecula Riverside - Tyler	Incomes are too low	
Chicks	Nearest	Not in area	Incomes are too low	
<u>Toys-Kids</u>				
Toys 'R Us/Kids	Nearest Also in	Moreno Valley Murrieta	Spacing not good	
KB Toys	Nearest Also in	Hemet Lake Elsinore	Spacing not good	
TOTAL SIGNIFICANT RETAILER SQ.FT.				550

Significant Tenant Opportunities Available to City of Perris: Development Potential Summary

- Potential Significant Tenant Opportunities - Sq. Ft. 580,000
- Total Potential Sq. Ft.
(If significant tenants = 80% of total Sq. Ft.) 725,000
- Total Potential Sq. Ft.
(If significant tenants = 67% of total Sq. Ft.) 865,000

The above equates to a range of between 70 to 85 acres. It does not take into account the empty space Wal-Mart will leave due to its move or the amount of existing under-utilized space that exists within the City.

Observations/Conclusions

- The City's significant tenant opportunities are relatively weak. This is due to the spacing of store locations in the region, income levels, and total population.
- Population within the realistic market area is not currently high enough to attract many quality retailers, especially with Moreno Valley/Canyon Springs to the north, Hemet to the east/southeast, and Murrieta/Temecula to the south.
- A few tenants who require smaller population levels should be interested in Perris in order to extend their market penetration.
- Realistically, the City of Perris could capture approximately 375,000 to 450,000 of additional retail space, not including the planned Wal-Mart Supercenter.
- There is insufficient additional demand to justify additional retail acreage along North Perris Boulevard (north of Nuevo Road).

Observations/Conclusions

- The existing Wal-Mart store will be vacated, adding to the supply of marginal retail space. Hopefully, Home Depot would take an aggressive position in this space (assuming Lowe's locates next to the new Wal-Mart Supercenter).
- The existing Wal-Mart center already has a marginal tenant mix. There is no solid reason to introduce additional retail between this center and the planned Trackman site.
- As more retail sites become available in the area, particularly those with freeway visibility and accessibility, interior sites like those along Perris Boulevard will become less desirable.
- Existing grocery stores in the City will be hit hard when the new Wal-Mart opens. At least one large grocery store will likely be forced to close as the market area cannot support significant additional grocery store space when existing markets are already performing below average.

Observations/Conclusions

- Population growth on the northeast portion of the market area will help somewhat.
- There is an over-supply of independent mom & pop markets in the City. This reflects the demographic profile of the City.
- Considering the relative lack of significant retail opportunities available to the City, and the lack of suitable space in/around your planned community, the City would be best served by pursuing retail development in locations that have the best chance to attract quality retailers.

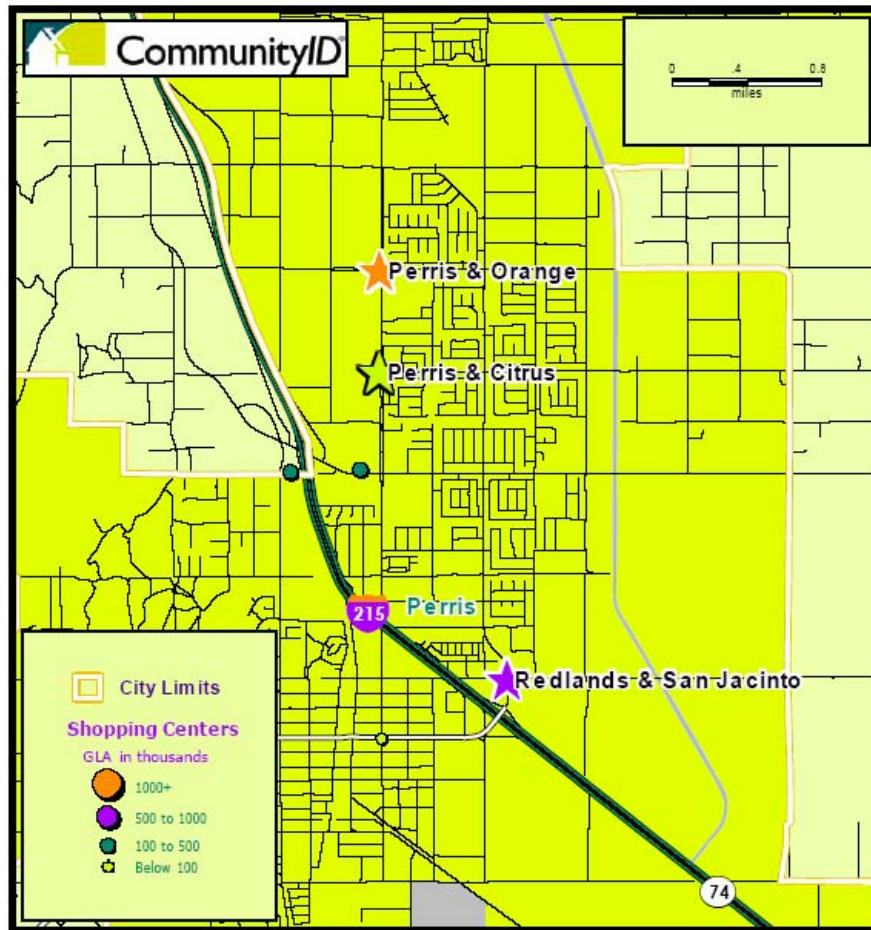
Potential Alternative Retail Sites

- Considering the relative lack of significant retail opportunities available to the City, and the lack of suitable space in/around your planned community, the City would be best served by pursuing retail development in locations that have the best chance to attract quality retailers.
- The City of Perris has tasked The Buxton Company to evaluate the potential for three potential retail sites within the City. These are identified on the following pages.
- As judged by The Buxton Group, the site located at the intersection of Redlands Avenue and San Jacinto Avenue has the best chance to attract quality retailers.
- The sites nearest the subject site at the cross-streets of Perris Boulevard and both Citrus Avenue and Orange Avenue are less desirable.

Project Overview

Perris, California

This map depicts the locations of the three retail sites that have been evaluated in Phase I.



Potential Alternative Retail Sites

The map to the left shows the locations of three potential retail sites studied by the Buxton Company for the City of Perris. These include:

1. Perris Blvd. & Orange Ave.
2. Redlands Ave. & San Jacinto Ave.
3. Perris Blvd. & Citrus Ave.

Site #2, at the southern part of town, was judged to be the best future retail site for the City.

The following map illustrates the locations of these three retail sites and existing/planned grocery stores.

Potential Retail Sites and Grocery Stores Located in Perris



	Grocery Chain Stores
	Cardenas Markets
	Food 4 Less
	Stater Bros Markets
	Wal-Mart Supercenter
	Smaller Markets
	Pushpins
	Smaller Markets

Comparison of Sites

10-Minute Trade Area Statistics **Trade Area Rank**

Site	Population	Population Growth	Household Count	Average Income	Residential Property Value	Traffic
Site 1	120,354 (3)	39.53 (3)	37,164 (3)	\$46,411 (3)	\$106,822 (3)	18,800 (2)
Site 2	126,858 (1)	41.29 (1)	40,671 (1)	\$48,119 (1)	\$112,759 (1)	63,000 (1)
Site 3	120,985 (2)	40.92 (2)	37,901 (2)	\$46,556 (2)	\$107,924 (2)	17,650 (3)

As shown above, Site #2 ranks first in each of six important site characteristics.

Site #3, located at Perris Boulevard and Orange Avenue ranks second in most site characteristics.

Observations/Conclusions

- Given the potential for other sites located south and north of the subject site, the City should concentrate its future retail development efforts in and around the intersection of Redlands/San Jacinto/I-215.
- Quality retailers will be attracted to the site due to its superior site attributes, including direct freeway accessibility and exposure.
- Sites located to the north of the subject site are less desirable and will be challenged to attract quality retailers, especially considering the current condition and tenant mix of existing centers near the Albertsons/Wal-Mart center.
- Simply put, the City does not need additional retail space along Perris Boulevard, especially retail space that will likely not perform up to retail industry standards.



APPENDIX D: ENVIRONMENTAL IMPACT REPORT

Under separate cover. Please see the City of Perris Department of Development Services.

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